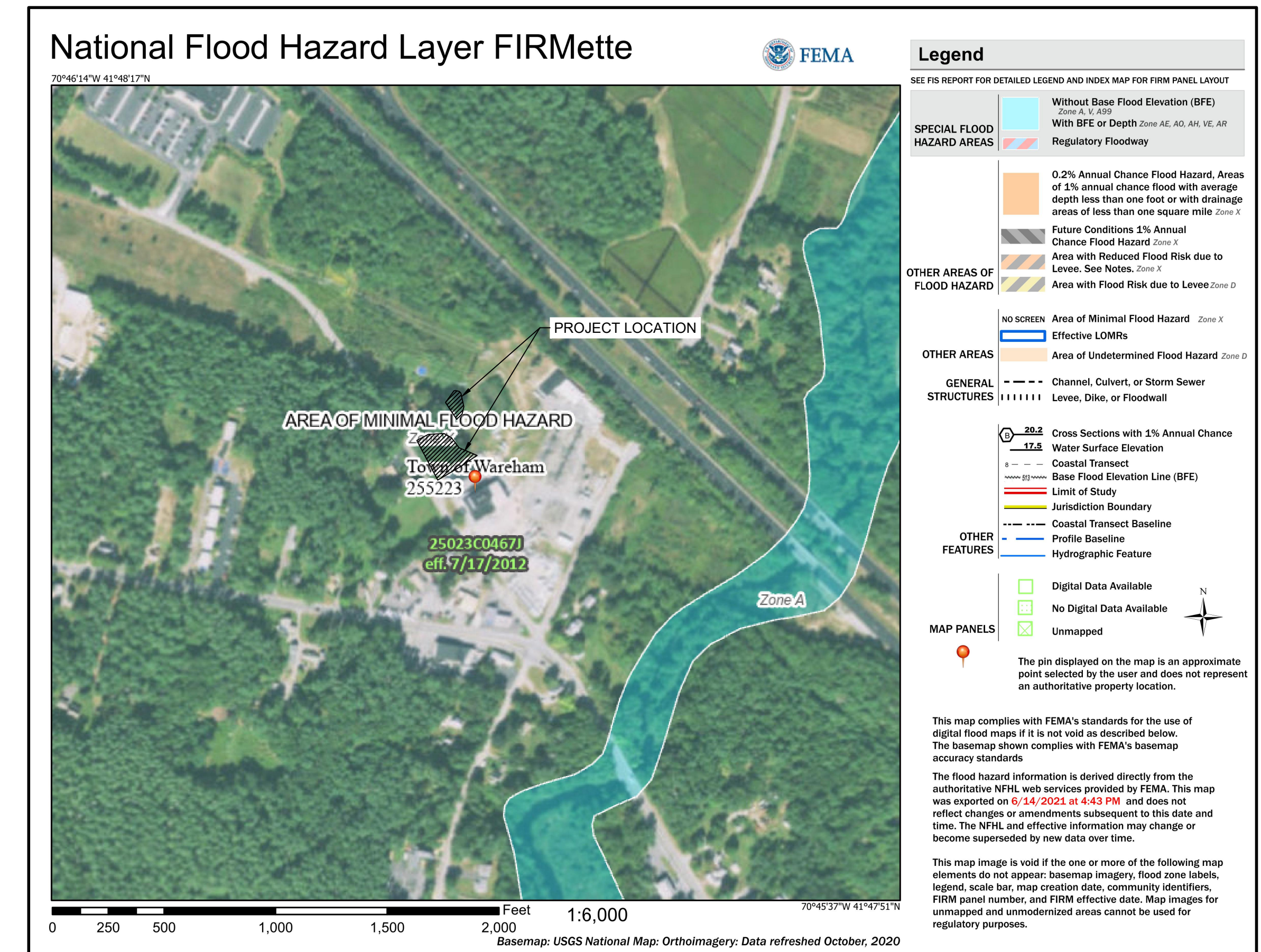


EVERSOURCE WAREHAM VEHICLE STORAGE LOT

DOTY STREET, WAREHAM, MA 02576



Sheet List Table	
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C000	Cover Sheet
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NORTH
FLOOD INSURANCE RATE MAP
NOT TO SCALE

DEVELOPER / OWNER:
EVERSOURCE ENERGY, INC.
5 DOTY STREET, WAREHAM, MA 02576

DATE OF APPLICATION: 07/05/2022

PROPERTY ZONING DISTRICT: CS - COMMERCIAL STRIP

GENERAL ON-SITE SOIL TYPES (PER NRCS) - CARVER LOAMY COARSE SAND (GROUP A) AND UDORTHENTS - URBAN LAND COMPLEX (GROUP A)



JEFF RATH, PE 61591 07/05/2022

EVERSOURCE WAREHAM
VEHICLE STORAGE LOT
EVERSOURCE ENERGY
DOTY STREET, WAREHAM, MA 02576

**PRELIMINARY
- NOT FOR
CONSTRUCTION**

DRAWN: EAW
APPROVED: JMR
ISSUED FOR: PERMIT RESUBMITTAL
DATE: 07/05/2022
PROJECT NO: 211970
FIELD BOOK: ---
CLIENT NO: ---

COVER SHEET

C000

A

B

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D

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GENERAL NOTES

1. CONTRACTOR TO CONFIRM THE HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES AND INFORM THE ENGINEER OF ANY POTENTIAL CONFLICTS WITH PROPOSED UTILITIES PRIOR TO CONSTRUCTION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR SITE SAFETY AND ALL WAYS, MEANS AND METHODS OF CONSTRUCTION.
3. CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL AGENCY CODES, STANDARDS AND SPECIFICATIONS.
4. CONTRACTOR SHALL OBTAIN ALL NECESSARY SITE PERMITS AND LICENSES FROM THE APPLICABLE GOVERNING AUTHORITIES.
5. ALL CONSTRUCTION MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE LATEST STATE AND LOCAL GOVERNMENT CONSTRUCTION STANDARDS AND SPECIFICATIONS.
6. UNLESS OTHERWISE NOTED ON THE PLANS, CONTRACTOR SHALL NOTIFY THE LOCAL ENGINEERING OR PUBLIC WORKS DEPARTMENT AND/OR OTHER PROJECT GOVERNING AUTHORITY(S) A MINIMUM OF FORTY-EIGHT (48) HOURS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS AND TO SCHEDULE ANY REQUIRED SITE INSPECTIONS.
7. CONTRACTOR SHALL SCHEDULE A UTILITY LOCATING SERVICE AND/OR NOTIFY ALL UTILITY COMPANIES (GAS, ELECTRIC, TELEPHONE, CABLE, ETC.) AND THE LOCAL MUNICIPALITY TO DETERMINE THE LOCATION OF UNDERGROUND UTILITIES PRIOR TO THE COMMENCEMENT OF CONSTRUCTION IN ORDER TO AVOID POTENTIAL CONFLICTS. IT IS ULTIMATELY THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL EXISTING UTILITIES WHETHER INDICATED ON THE PLANS OR NOT AND TO HAVE THESE UTILITIES STAKED PRIOR TO CONSTRUCTION. ANY NECESSARY RELOCATIONS OR REMOVALS OF EXISTING UTILITY LINES SHALL BE PERFORMED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
8. CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL PRIVATE AND PUBLIC UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT HIS EXPENSE AND TO THE SATISFACTION OF THE UTILITY OWNER.
9. ALL EASEMENTS FOR EXISTING UTILITIES, BOTH PUBLIC AND PRIVATE, AND UTILITIES WITHIN PUBLIC RIGHTS-OF-WAY ARE SHOWN ON THE PLANS PREPARED BY THE SURVEYOR ACCORDING TO INFORMATION AVAILABLE FROM PUBLIC RECORDS OR VISIBLE FIELD MARKINGS. THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE FOR DETERMINING THE EXACT LOCATION IN THE FIELD OF THESE UTILITY LINES AND FOR THEIR PROTECTION FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH THE PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER SO THE CONFLICT MAY BE RESOLVED.
10. ALL UTILITY CONNECTIONS TO EXISTING LINES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE RULES AND REGULATIONS AND TO THE SATISFACTION OF THE APPLICABLE UTILITY OWNER(S).
11. CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS, COORDINATES AND ELEVATIONS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION AND SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES SO THE CONFLICT MAY BE RESOLVED.
12. ALL PROPERTY MARKERS AND SURVEY REFERENCE MARKERS SHALL BE CAREFULLY PRESERVED DURING CONSTRUCTION UNTIL THEIR LOCATION HAS BEEN WITNESSED OR OTHERWISE TIED IN BY AN AUTHORIZED AGENT OR PROFESSIONALLY LICENSED SURVEYOR.
13. THE SAFE AND ORDERLY PASSAGE OF TRAFFIC AND PEDESTRIANS SHALL BE PROVIDED WHERE CONSTRUCTION OPERATIONS ABUT PUBLIC THROUGH-FARES AND ADJACENT PROPERTY.
14. ALL AREAS DISTURBED BY THE GENERAL CONTRACTOR OR SUB-CONTRACTORS SHALL BE RETURNED TO THE ORIGINAL CONDITION OR BETTER, EXCEPT WHERE PROPOSED CONSTRUCTION IS INDICATED ON THE PLANS.
15. PRIOR TO INITIAL ACCEPTANCE BY THE OWNER(S) AND/OR GOVERNING AUTHORITY, ALL WORK SHALL BE INSPECTED AND APPROVED BY THE OWNER AND MUNICIPALITY ENGINEER OR HIS REPRESENTATIVE(S). THE CONTRACTOR SHALL GUARANTEE HIS WORK FOR A PERIOD OF 12 (TWELVE) MONTHS FROM THE DATE OF SUBSTANTIAL COMPLETION AND SHALL BE HELD RESPONSIBLE FOR ANY DEFECTS IN MATERIAL OR WORKMANSHIP OF THIS WORK DURING THAT PERIOD AND UNTIL FINAL ACCEPTANCE IS MADE.
16. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFE AND ADEQUATE WORKING CONDITIONS THROUGHOUT THE DURATION OF CONSTRUCTION OF THE PROPOSED IMPROVEMENTS.
17. CONTRACTOR SHALL KEEP THE PUBLIC STREET PAVEMENTS CLEAN OF DIRT AND DEBRIS AND, WHEN NECESSARY, CLEAN PAVEMENTS AT THE END OF EACH WORKING DAY.
18. ALL CONSTRUCTION STAKING, SCHEDULING AND PAYMENT IS THE RESPONSIBILITY OF THE CONTRACTOR.
19. AFTER COMPLETION OF THE PROPOSED IMPROVEMENTS AND WHEN REQUIRED BY THE GOVERNING AUTHORITY(S), CONTRACTOR SHALL PROVIDE THE OWNER AND ENGINEER WITH AS-BUILT AND/OR RECORD DRAWINGS, SIGNED AND SEALED BY A PROFESSIONALLY LICENSED ENGINEER OR SURVEYOR AND SHALL INCLUDE AT A MINIMUM (WHERE APPLICABLE TO THE SCOPE OF WORK) THE FOLLOWING ITEMS:
 - 19.1. TOPOGRAPHY AND SPOT GRADE ELEVATIONS OF ALL PROPOSED PERMANENT SITE FEATURES INCLUDING ANY STORM WATER FACILITIES OR MODIFICATIONS TO EXISTING STORM WATER FACILITIES.
 - 19.2. HORIZONTAL AND VERTICAL LOCATION AND ALIGNMENT OF ALL PROPOSED ROADWAYS, PARKING LOTS, UTILITIES, BUILDINGS OR OTHER PERMANENT SITE FEATURES.
 - 19.3. RIM AND INVERT AND/OR TOP OF PIPE ELEVATIONS FOR ALL PROPOSED UTILITIES.
 - 19.4. AS-BUILT AND/OR RECORD DRAWING INFORMATION SHALL BE SHOWN ON THE APPROVED ENGINEERING PLANS ISSUED FOR CONSTRUCTION. ANY AND ALL DEVIATIONS FROM THESE APPROVED PLANS SHALL BE SHOWN BY MEANS OF STRIKING THROUGH THE PROPOSED INFORMATION AND CLEARLY INDICATING THE AS-BUILT LOCATIONS AND ELEVATIONS ON THE APPLICABLE PLAN SHEET.
20. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF ALL SITE SETBACKS, EASEMENTS AND DIMENSIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION.
21. ALL HANDICAP ACCESSIBLE SITE IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH ALL FEDERAL, STATE AND LOCAL CODES AND REQUIREMENTS.
22. IF DURING THE COURSE OF CONSTRUCTION THE CONTRACTOR FINDS ANY DISCREPANCIES OR CONFLICTS BETWEEN THE PROPOSED SITE IMPROVEMENTS INDICATED ON THE PLANS AND THE PHYSICAL CONDITIONS OF THE SITE, OR ANY ERRORS OR OMISSIONS WITHIN THE PLANS OR IN THE SITE LAYOUT AS PROVIDED BY THE ENGINEER, IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO IMMEDIATELY NOTIFY THE ENGINEER. UNTIL AUTHORIZED TO PROCEED, ANY WORK PERFORMED BY THE CONTRACTOR AFTER SUCH A DISCOVERY WILL BE AT THE CONTRACTOR'S SOLE RISK AND EXPENSE.
23. CONTRACTOR SHALL COORDINATE ALL SITE IMPROVEMENTS WITH ARCHITECTURAL PLANS. ARCHITECTURAL PLANS SHALL BE USED FOR BUILDING STAKEOUT.
24. CONTRACTOR SHALL COORDINATE ALL LANDSCAPE IMPROVEMENTS, INCLUDING NEW PLANTINGS AND TURF AREA RESTORATION REQUIREMENTS, WITH LANDSCAPE PLANS.
25. ALL DIMENSIONS SHOWN ARE MEASURED FROM FACE OF CURB TO FACE OF CURB OR EDGE OF PAVEMENT TO EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
26. ALL CURB RADII ARE MEASURED AT THE FACE OF CURB UNLESS OTHERWISE NOTED.
27. ALL NEW ASPHALT AND/OR CONCRETE PAVING SHALL MATCH EXISTING PAVEMENTS FLUSH.
28. CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS OUTSIDE OF CONSTRUCTION LIMITS TO ORIGINAL CONDITION OR BETTER.
29. CONTRACTOR SHALL REPAIR AT HIS EXPENSE ANY DAMAGE TO EXISTING ASPHALT, CONCRETE, CURBS, SIDEWALKS, ETC. RESULTING FROM CONSTRUCTION TRAFFIC AND/OR OPERATIONS. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE OWNER AND/OR ENGINEER.
30. ALL FIRE ACCESS LANES WITHIN THE PROJECT AREA SHALL REMAIN IN SERVICE, CLEAN OF DEBRIS, AND ACCESSIBLE FOR USE BY EMERGENCY VEHICLES.
31. ALL DETECTABLE WARNING PLATES SHALL BE PREFORMED PLASTIC INSERTS UNLESS OTHERWISE NOTED.

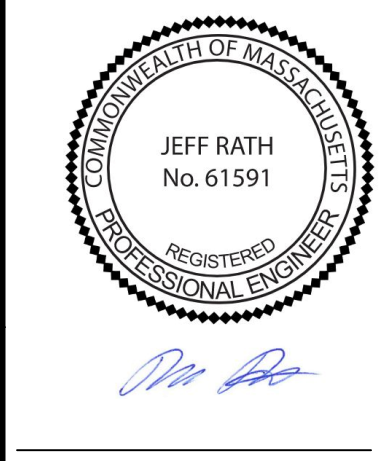
SITE GRADING & PAVING NOTES

1. ALL SITE WORK, GRADING, AND PAVING OPERATIONS WITHIN THE LIMITS OF THE PROJECT SHALL BE PERFORMED IN ACCORDANCE WITH THE NOTES IN THE PLANS AND THE STANDARDS, SPECIFICATIONS, CODES AND ORDINANCES OF THE LOCAL GOVERNING AUTHORITIES. IN CASE OF CONFLICT, THE MORE STRINGENT CODE SHALL TAKE PRECEDENCE.
2. EARTH EXCAVATION SHALL INCLUDE CLEARING, STRIPPING AND STOCKPILING TOPSOIL, REMOVING UNSUITABLE MATERIALS, CONSTRUCTION OF EMBANKMENTS, NON-STRUCTURAL FILLS, FINAL SHAPING AND TRIMMING TO THE LINES, GRADES AND CROSS SECTIONS SHOWN ON THE PLANS. ALL UNSUITABLE OR EXCESS MATERIAL SHALL BE DISPOSED OF LEGALLY OFFSITE OR AS DIRECTED BY THE PROJECT REPRESENTATIVE IN THE FIELD.

3. EXCAVATED TOPSOIL SHALL BE STOCKPILED ON THE SITE IN AREAS DESIGNATED BY THE PROJECT ENGINEER UNTIL SUCH TIME THAT THIS TOPSOIL CAN BE USED FOR FINAL GRADING. UNLESS OTHERWISE NOTED ON THE PLANS, A MINIMUM OF 6" TOPSOIL RE-SPREAD AND SEEDING FOR ALL DISTURBED AREAS IS REQUIRED.
4. THE GEOTECHNICAL INVESTIGATION REPORT FOR THE SITE AND ALL ADDENDA THERETO ARE SUPPORTING DOCUMENTS FOR THIS PROJECT. THE RECOMMENDATIONS AS STATED IN SAID REPORT ARE HEREBY INCORPORATED INTO THESE CONSTRUCTION NOTES BY REFERENCE AND SHALL BE FOLLOWED BY ALL CONTRACTORS. THE GRADING OPERATIONS ARE TO BE CLOSELY SUPERVISED AND INSPECTED, PARTICULARLY DURING THE REMOVAL OF UNSUITABLE MATERIAL AND THE CONSTRUCTION OF EMBANKMENTS OR BUILDING PADS. BY A SOILS ENGINEER OR HIS REPRESENTATIVE. FURTHER CONSTRUCTION OPERATIONS WILL NOT BE PERMITTED UNTIL THE SOILS ENGINEER ISSUES A WRITTEN STATEMENT THAT THE AREA IN QUESTION HAS BEEN SATISFACTORILY PREPARED AND IS READY FOR CONSTRUCTION.
5. ALL TESTING, INSPECTION AND SUPERVISION OF SOIL QUALITY, UNSUITABLE SOIL REMOVAL AND ITS REPLACEMENT AND OTHER SOILS RELATED OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE COORDINATED WITH AND PERFORMED AT THE DIRECTION OF THE OWNER'S GEOTECHNICAL ENGINEER.
6. THE CONTRACTOR SHALL USE CARE IN GRADING NEAR TREES, SHRUBS, AND BUSHES WHICH ARE NOT NOTED TO BE REMOVED SO AS NOT TO CAUSE INJURY TO ROOTS OR TRUNKS.
7. THE CONTRACTOR SHALL USE CARE IN GRADING OR EXCAVATING NEAR ANY AND ALL EXISTING ITEMS WHICH ARE NOT INDICATED TO BE REMOVED. ANY DAMAGE DONE TO THESE EXISTING ITEMS BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED AT HIS OWN EXPENSE.
8. REMOVED PAVEMENTS, SIDEWALKS, CURBS, TREES AND STUMPS SHALL BE DISPOSED OF LEGALLY OFFSITE AT LOCATIONS DETERMINED BY THE CONTRACTOR.
9. ON AND OFFSITE PAVING AND CURBS TO REMAIN SHALL BE PROTECTED FROM DAMAGE, AND, IF DAMAGED, SHALL BE REPLACED PROMPTLY TO MEET STATE AND LOCAL STANDARD SPECIFICATIONS IN MATERIALS AND WORKMANSHIP.
10. PROPOSED ELEVATIONS INDICATE FINISHED GRADE CONDITIONS. FOR ROUGH GRADING ELEVATIONS ALLOW FOR THE THICKNESS OF THE PROPOSED PAVEMENT SECTION (ROADS, WALKS, DRIVE, ETC.) OR TOPSOIL AS INDICATED ON THE PLANS.
11. CONTRACTOR SHALL PROVIDE SMOOTH VERTICAL CURVES THROUGH THE HIGH AND LOW POINTS INDICATED BY SPOT ELEVATIONS ON THE PLANS. CONTRACTOR SHALL PROVIDE UNIFORM SLOPES BETWEEN NEW AND EXISTING GRADES AND AVOID ANY RIDGES AND/OR DEPRESSIONS.
12. ALL PROPOSED GRADING, PAVEMENT, APRONS, CURBS, WALKS, ETC. SHALL MATCH EXISTING GRADES FLUSH.
13. ALL EXISTING AND PROPOSED TOP OF FRAME ELEVATIONS FOR STORM, SANITARY, WATER AND OTHER UTILITY STRUCTURES SHALL BE ADJUSTED TO MEET FINISHED GRADE WITHIN THE PROJECT LIMITS.
14. REQUIRED RIM ADJUSTMENTS SHALL BE MADE WITH PRECAST CONCRETE ADJUSTING RINGS NOT TO EXCEED A MAXIMUM OF EIGHT (8) INCHES IN OVERALL HEIGHT. A MAXIMUM OF TWO (2) ADJUSTING RINGS ARE ALLOWED. BUTYROPE JOINT SEALANT SHALL BE USED ON ALL JOINTS BETWEEN THE PRECAST ELEMENTS.
15. SITE GRADING AND CONSTRUCTION OF THE PROPOSED SITE IMPROVEMENTS SHALL NOT CAUSE PONDING OF STORM WATER. ALL AREAS ADJACENT TO THESE IMPROVEMENTS SHALL BE GRADED TO ALLOW POSITIVE DRAINAGE AND MATCH EXISTING GRADES FLUSH.
16. CONTRACTOR SHALL ENSURE POSITIVE SITE DRAINAGE AT THE END OF EACH WORKING DAY DURING CONSTRUCTION OPERATIONS. FAILURE TO PROVIDE ADEQUATE DRAINAGE WILL PRECLUDE THE CONTRACTOR FROM ANY POSSIBLE COMPENSATION REQUESTED DUE TO DELAYS OR UNSUITABLE MATERIALS CREATED AS A RESULT.
17. DRIVEWAYS SHALL BE CONSTRUCTED SO AS NOT TO IMPEDE THE SURFACE DRAINAGE SYSTEM.
18. TRAFFIC CONTROL DEVICES SHALL BE IN CONFORMANCE WITH THE APPLICABLE STATE DEPARTMENT OF TRANSPORTATION STANDARDS AND SHALL BE INSTALLED AND PROVIDED WHENEVER CONSTRUCTION FOR UTILITIES ARE WITHIN STREET AREAS. APPLICABLE ORDINANCES OF THE MUNICIPALITY, COUNTY OR STATE SHALL ALSO GOVERN THE TRAFFIC CONTROL REQUIREMENTS.
19. ALL GRADING AND SITE PREPARATION WORK SHALL CONFORM WITH THE RECOMMENDATIONS AND SPECIFICATIONS CONTAINED IN THE GEOTECHNICAL REPORT.
20. CONTRACTOR SHALL CAREFULLY PRESERVE ALL SITE BENCHMARKS AND REFERENCE POINTS DURING CONSTRUCTION OPERATIONS.
21. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES AND, WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE LOCAL UTILITY LOCATION CENTER AT LEAST FORTY-EIGHT (48) HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATIONS OF THE UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED SITE IMPROVEMENTS SHOWN ON THE PLANS.
22. CONTRACTOR SHALL INSTALL APPROPRIATE TREE PROTECTION MEASURES PRIOR TO COMMENCEMENT OF SITE GRADING OPERATIONS.
23. CONTRACTOR SHALL UTILIZE CARE WHEN WORKING NEAR EXISTING UTILITIES TO REMAIN. ANY DAMAGE TO EXISTING UTILITIES NOT NOTED TO BE REMOVED SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE OWNER AND/OR ENGINEER.
24. CONTRACTOR SHALL REPAIR AT HIS EXPENSE ANY DAMAGE TO EXISTING ASPHALT, CONCRETE, CURBS, SIDEWALKS, ETC. RESULTING FROM CONSTRUCTION TRAFFIC AND/OR OPERATIONS. REPAIRS SHALL BE MADE TO THE SATISFACTION OF THE OWNER AND/OR ENGINEER.
25. CONTRACTOR SHALL RESTORE ALL DISTURBED AREAS OUTSIDE OF CONSTRUCTION LIMITS TO ORIGINAL CONDITION OR BETTER.
26. MAXIMUM CROSS SLOPES AND LONGITUDINAL SLOPES FOR ALL CONCRETE SIDEWALKS AND HANDICAP ACCESSIBLE ROUTES SHALL NOT EXCEED 2% AND 5%, RESPECTIVELY.
27. MAXIMUM SLOPES WITHIN THE HANDICAP ACCESSIBLE PARKING AREAS SHALL NOT EXCEED 2% IN ANY DIRECTION.
28. MAXIMUM GRADE DIFFERENCE BETWEEN PAVED SURFACES AND ADJACENT CONCRETE SIDEWALKS FOR THE ACCESSIBLE ROUTE TO THE BUILDING SHALL NOT EXCEED 1/4" VERTICAL OR 1/2" WHEN BEVELED.
29. ALL HANDICAP ACCESSIBLE EXTERIOR DOORWAY LOCATIONS REQUIRE AN EXTERIOR LANDING THAT IS A MINIMUM OF FIVE (5) FEET IN LENGTH WITH A SLOPE NOT EXCEEDING 2% IN ANY DIRECTION.
30. EXCAVATION SHORING SHALL BE DONE AS NECESSARY FOR THE PROTECTION OF THE WORK AND FOR THE SAFETY OF PERSONNEL. SHORING SHALL BE IN ACCORDANCE WITH ALL O.S.H.A. AND LOCAL REGULATIONS.
31. ALL STRUCTURE BENCH WALLS SHALL BE SHAPED AND FORMED FOR A CLEAN TRANSITION WITH PROPER HYDRAULICS TO ALLOW THE SMOOTH CONVEYANCE OF FLOWS THROUGH THE MANHOLE OR BOX INLET. THE BENCH WALL SHALL FORM A DEFINED CHANNEL, TO A MINIMUM HEIGHT OF 80-PERCENT OF THE INSIDE DIAMETER OF THE INLET AND OUTLET PIPES TO FORM A "U" SHAPED CHANNEL, CONSTRUCTED AT A MINIMUM 1/4-INCH PER FOOT SLOPE TO THE MANHOLE WALL.
32. ALL STORM WATER INLETS AND CATCH BASIN CASTINGS SHALL HAVE THE WORDS "NO DUMPING, DRAINS TO STREAM", OR SIMILARLY APPROVED MESSAGE, CAST IN RAISED OR RECESSED LETTERS AT A MINIMUM OF 1" IN HEIGHT. IN ADDITION, A SYMBOL OF A FISH SHALL ALSO BE CAST WITH THE LETTERS.

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SHIVEHATTERY
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JEFF RATH, PE 61591 07/05/2022

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**CIVIL GENERAL
NOTES & LEGENDS**

C001

PAVEMENT GENERAL NOTES

- LIMIT OPERATIONS TO THE PUBLIC RIGHT-OF-WAYS AND EASEMENTS OR INDICATED PROJECT LIMITS. THE CONTRACTOR SHALL NOT UTILIZE PRIVATE PROPERTY UNLESS HE HAS SUBMITTED A COPY OF THE PROPERTY OWNER'S WRITTEN PERMISSION TO THE OWNER'S REPRESENTATIVE.
- ALL SLOPES IN PAVEMENT SHALL BE UNIFORM TO AVOID PONDING.
- REMOVE AND REPLACE OR RESTORE ALL STREET SIGNS, PAVEMENT MARKINGS, SIDEWALK LAMPS, SIDEWALKS, STEPS, LANDSCAPE STRUCTURES, CURB AND GUTTER, STREETS, DRIVES, AND ALL OTHER SURFACE STRUCTURES REMOVED OR OTHERWISE DAMAGED DURING THE COURSE OF THE WORK. SIDEWALKS SHALL BE REMOVED AND REPLACED TO NEAREST JOINT BEYOND CONSTRUCTION AREA.
- COMPACT SUBGRADE BENEATH PAVEMENTS IN ACCORDANCE WITH GRADING NOTES.

SURFACE RESTORATION

- ALL DISTURBED AREAS NOT PAVED OR HARD SURFACE ON THE SITE SHALL RECEIVE 6 INCHES OF TOPSOIL. SCARIFY AREAS TO RECEIVE TOPSOIL TO A DEPTH OF 3 INCHES. REMOVE ALL STONES, WOOD, AND OTHER DEBRIS LARGER THAN 2 INCHES FROM AREAS TO RECEIVE TOPSOIL. DO NOT COMPACT TOPSOIL.
- ALL DISTURBED AREAS SHALL BE SEEDED, FERTILIZED, AND MULCHED IN ACCORDANCE WITH THE PROVIDED LANDSCAPING PLAN, NOTES, AND DETAILS.
- REFER TO LANDSCAPE PLAN FOR PLANTING INSTRUCTIONS AND DETAILS.
- MAINTAIN ANY SEEDED AREAS UNTIL AND ADEQUATE STAND OF GRASS HAS BEEN ESTABLISHED. RESEED ANY AREAS AS NECESSARY DURING MAINTENANCE PERIOD.

EROSION CONTROL NOTES

- USE ALL AVAILABLE MEANS TO MINIMIZE THE AMOUNT OF SOIL EROSION CAUSED BY THE WORK OF THE PROJECT. THIS INCLUDES RESPONSIBILITY FOR MAINTENANCE OF ALL SOIL EROSION CONTROL DEVICES INDICATED. REPAIRS TO SOIL EROSION CONTROL DEVICES SHALL BE COMPLETED WITHIN 2 DAYS FROM NOTIFICATION.
- EROSION CONTROL DEVICES SHALL BE CHECKED ONCE PER WEEK AND AFTER EACH RAINFALL TO ENSURE WORKING ORDER.
- SILT FENCE SHALL BE LOCATED AS SHOWN ON THE PLANS.
- SEED ALL DISTURBED AREAS EXPOSED FOR MORE THAN 21 DAYS WITH A STABILIZING CROP.

UTILITY NOTE

THE LOCATIONS OF UTILITY MAINS, STRUCTURES, AND SERVICE CONNECTIONS PLOTTED ON THIS DRAWING ARE APPROXIMATE ONLY AND WERE OBTAINED FROM RECORDS MADE AVAILABLE TO SHIVE-HATTERY, INC. THERE MAY BE OTHER EXISTING UTILITY MAINS, STRUCTURES, AND SERVICE CONNECTIONS NOT KNOWN TO SHIVE-HATTERY, INC., AND NOT SHOWN ON THIS DRAWING.

SURVEY	
PLAN MARK	DESCRIPTION
	BENCH MARK
	CONTROL POINT
	IRON ROD - FOUND
	IRON ROD - SET
	SECTION CORNER FOUND
	MONUMENT SET
	X CUT FOUND
	X CUT SET
	RIGHT OF WAY MARKER
	NAIL FOUND
	NAIL SET
	STATION MARKER
	SOIL BORING
	RECORD BEARING/DISTANCE
	MEASURED BEARING/DISTANCE
	POINT OF BEGINNING
	POINT OF REFERENCE

GENERAL SITE	
PLAN MARK	DESCRIPTION
	EXISTING STRUCTURE
	GUARD POST/ BOLLARD
	MAILBOX
	PARKING METER
	FLAGPOLE
	HANDICAPPED PARKING
	SHRUB
	DECIDUOUS TREE
	CONIFEROUS TREE
	SINGLE POLE SIGN
	DOUBLE POLE SIGN
	TRAFFIC SIGNAL WITH ARM
	TRAFFIC SIGNAL
	UTILITY MARKER
	RAILROAD CROSSING SIGNAL
	TRAFFIC MANHOLE
	WIRE FENCE
	WOOD FENCE
	CHAINLINK FENCE
	RAILROAD
	GUARD RAIL
	TREE LINE
	MINOR CONTOUR
	MAJOR CONTOUR

UTILITY LINES		
EXISTING LINE TYPE	DESCRIPTION	PROPOSED LINE TYPE
	ELECTRIC - OVERHEAD	
	ELECTRIC - UNDERGROUND	
	GAS MAIN	
	WATER MAIN	
	SANITARY SEWER	
	SANITARY FORCE MAIN	
	STORM SEWER	
	TELEPHONE - OVERHEAD	
	TELEPHONE - UNDERGROUND	
	CABLE LINE - OVERHEAD	
	CABLE LINE - UNDERGROUND	
	FIBER OPTICS	
	PROCESS/HEATING STEAM	

UTILITIES	
PLAN MARK	DESCRIPTION
	FLOODLIGHT
	LIGHT POLE OVERHANG
	LIGHT POST
	SIREN POLE
	UTILITY POLE
	GUY ANCHOR
	UTILITY POLE W/ TRANSFORMER
	FIRE HYDRANT
	FLARED END SECTION
	UTILITY END CAP
	VALVE
	POST INDICATOR VALVE
	CABLE TV PEDESTAL
	CLEANOUT
	JUNCTION BOX
	FIBER OPTIC BOX
	MANHOLE
	DRAINAGE MANHOLE
	ELECTRICAL MANHOLE
	SANITARY MANHOLE
	TELEPHONE MANHOLE
	TELEPHONE PEDESTAL
	VAULT BOX
	MONITORING WELL
	WATER MANHOLE
	WATER IRRIGATION VALVE
	WATER SHUTOFF VALVE
	WATER METER
	HANDHOLE
	SIGNAL BOX
	GAS METER
	ELECTRIC METER
	TRANSFORMER
	AIR CONDITIONER
	CURB INLET
	INTAKE - CIRCLE
	INTAKE - RECTANGLE
	INTAKE - SQUARE
	RA-3 INTAKE
	RA-5 INTAKE
	RA-8 INTAKE
	YARD HYDRANT
	FIRE DEPARTMENT VALVE
	GAS REGULATOR
	ROOF DOWNSPOUT

GENERAL SITE DESIGN	
PLAN MARK	DESCRIPTION
	SPOT ELEVATION
	TOP OF CURB AND GUTTER ELEVATION
	SLOPE ARROW
	FLOW ARROW
	TRAFFIC FLOW
	SILT FENCE
	LIMITS
	LEFT-TURN ARROW
	RIGHT-TURN ARROW
	THRU ARROW
	LEFT/ THRU ARROW
	RIGHT/ THRU ARROW
	LEFT/ RIGHT/ THRU ARROW



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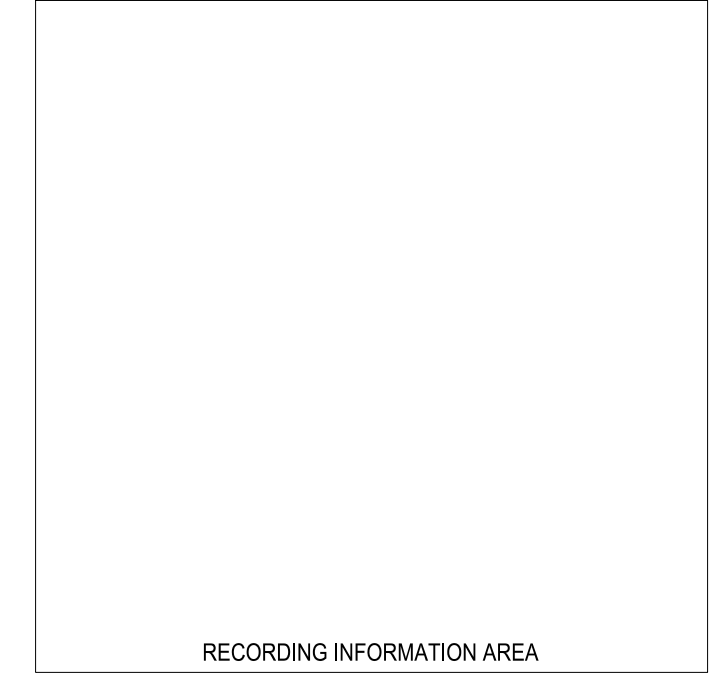
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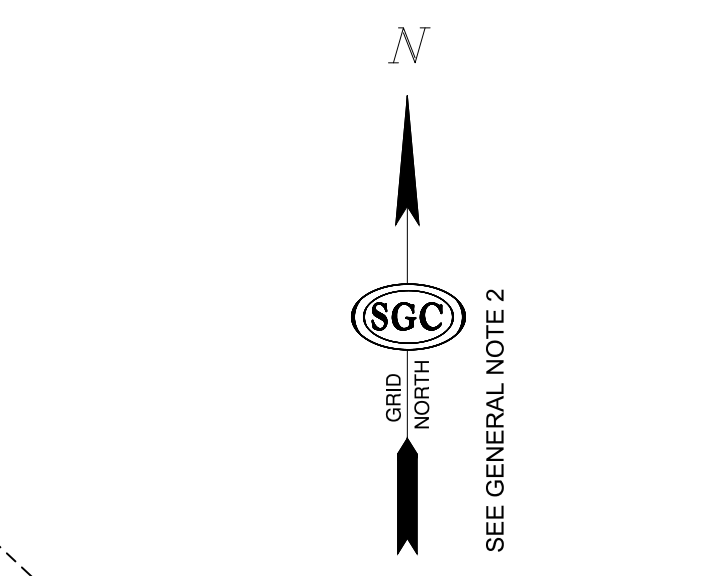
CIVIL GENERAL
NOTES & LEGENDS

C002

OWNER INFORMATION				
OWNER NO.	OWNER OF RECORD	BOOK/PAGE (P.C.R.D.)	TAX MAP, LOT NO.	AREA (ACRES)
1	NSTAR ELECTRIC COMPANY (N/F NEW BEDFORD GAS AND EDISON LIGHT COMPANY)	3895/339	MAP 103, LOT 1028	1.258
2	NSTAR ELECTRIC COMPANY (N/F FLYMOUTH COUNTY ELECTRIC COMPANY)	2079/115	MAP 103, LOT 1031	0.584
3	NSTAR ELECTRIC COMPANY	48795/265	MAP 104, LOT 1002	1.423
4	NSTAR ELECTRIC COMPANY (N/F NEW BEDFORD GAS AND EDISON LIGHT COMPANY)	3330/31	MAP 103, LOT 1027	4.132
5	NSTAR ELECTRIC COMPANY (N/F NEW BEDFORD GAS AND EDISON LIGHT COMPANY)	1742/440	MAP 103, LOT 1030	2.105
6	NSTAR ELECTRIC COMPANY (N/F COMMONWEALTH ELECTRIC COMPANY)	7123/243	MAP 103, LOT 1029	0.952
7	STEAMSHIP ASSOCIATES, LLC	23883/348	MAP 103, LOT A1	N/A
8	GRANVILLE E. STRINGER & JUDITH S. STRINGER	6972/180	MAP 103, LOT 1016A	N/A
9	CHARLES H. SIMPSON, III & ROXANNA SIMPSON	7316/320 (PARCEL 3)	MAP 104, LOT 1001	N/A
10	CHARLES H. SIMPSON, III & ROXANNA SIMPSON	7316/320 (PARCEL 1)	MAP 104, LOT 1000	N/A
11	WALDO ROUNDS, JR. & CAROL ROUNDS	3634/344	MAP 104, LOT 1003	N/A

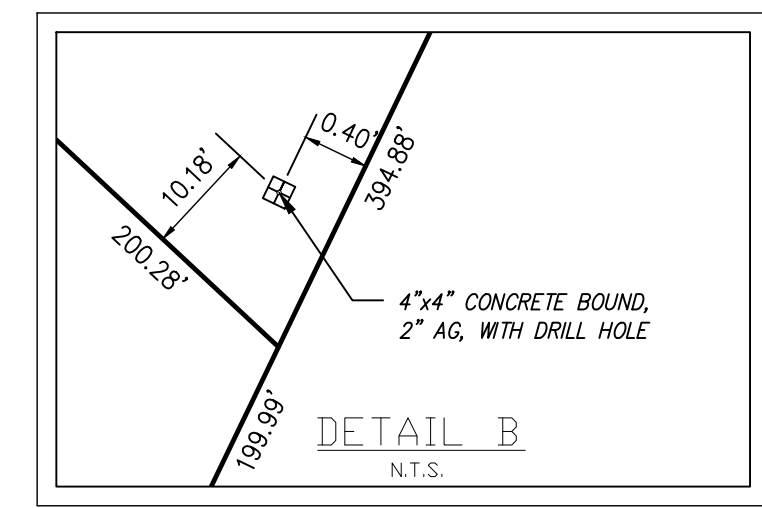
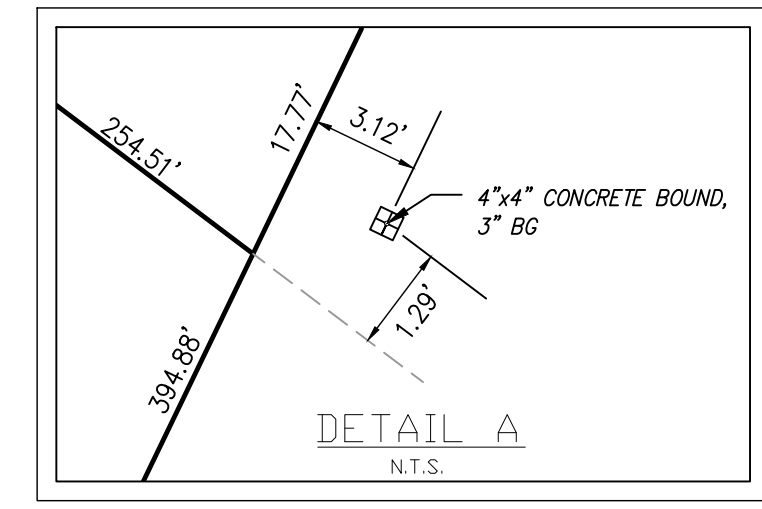
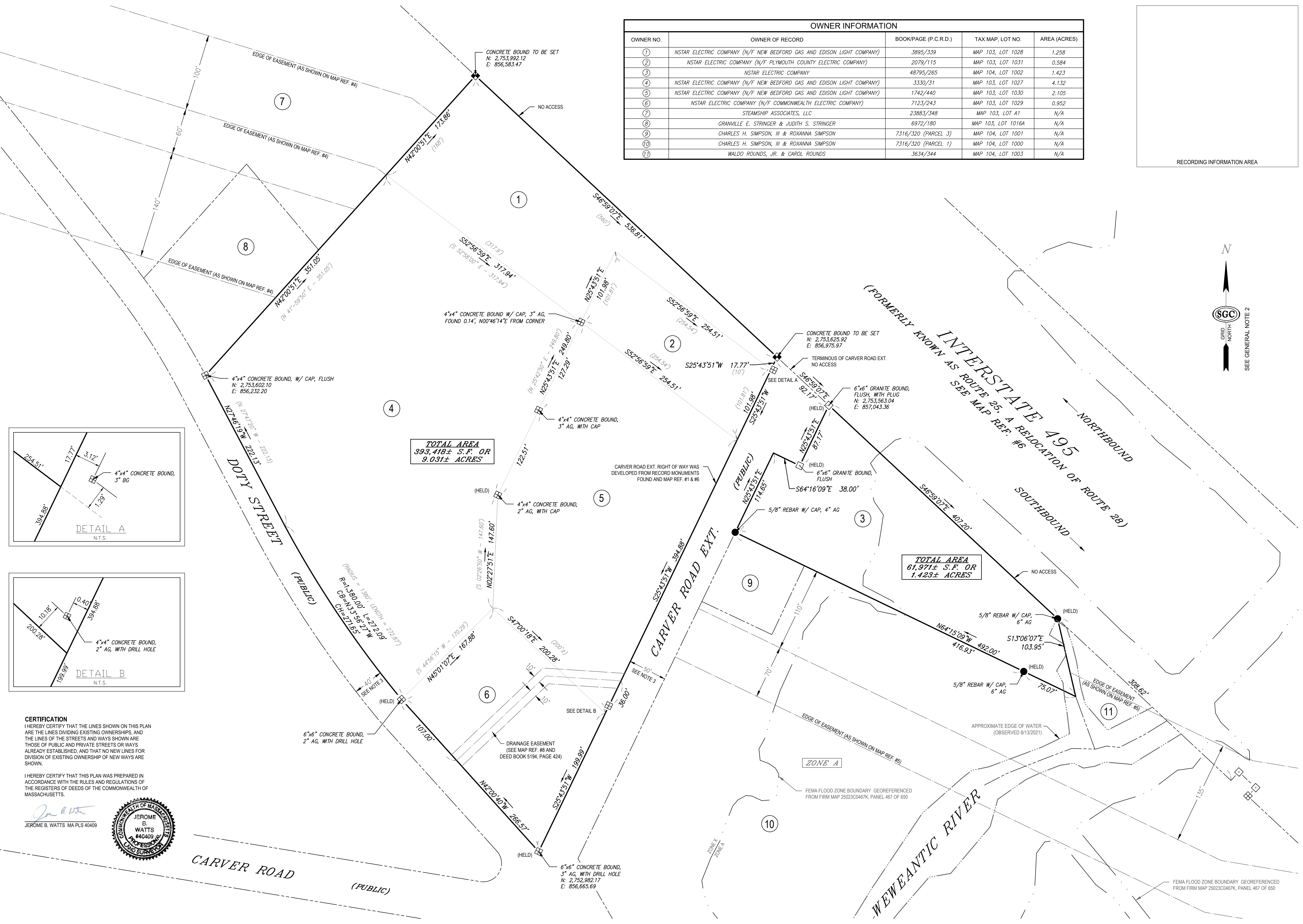


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10	CHARLES H. SIMPSON, III & ROXANNA SIMPSON	7316/320 (PARCEL 1)	MAP 104, LOT 1000	N/A
11	WALDO ROUNDS, JR. & CAROL ROUNDS	3634/344	MAP 104, LOT 1003	N/A



- GENERAL NOTES:**
- THIS PLAN AND ALL WORK ASSOCIATED WITH IT WAS PERFORMED BY SGC ENGINEERING, LLC PURSUANT TO A PROFESSIONAL SERVICES CONTRACT BETWEEN EVERSOURCE ENERGY AND SGC ENGINEERING LLC, DATED JUNE 28, 2021.
 - THE BEARINGS SHOWN HEREON REFER TO MASSACHUSETTS STATE PLANE COORDINATE SYSTEM, NORTH AMERICAN DATUM OF 1983 (2011) AND ARE BASED ON GPS MEASUREMENTS. ELEVATIONS ARE REFERENCED TO NAVD 88.
 - CARVER ROAD EXTENSION IS 50' WIDE PER MAP REFERENCE #1. DOTY STREET IS 40' WIDE PER MAP REFERENCE #9.
 - NO SUBSURFACE INVESTIGATION HAS BEEN PERFORMED BY SGC ENGINEERING, LLC. DIG-SAFE SHOULD BE CONTACTED PRIOR TO COMMENCING ANY EXCAVATION. (888-344-7233).
 - THIS PLAN IS THE RESULT OF A FIELD SURVEY CONDUCTED BY SGC ENGINEERING, LLC IN JULY/AUGUST 2021.
 - THE BOUNDARY IS SUBJECT TO CHANGE ALONG THE WEWEANTIC RIVER AND ITS TRIBUTARIES DUE TO NATURAL CAUSES THAT MAY OR MAY NOT REPRESENT THE ACTUAL LOCATION BASED ON DATE OF SURVEY.

- MAP REFERENCES:**
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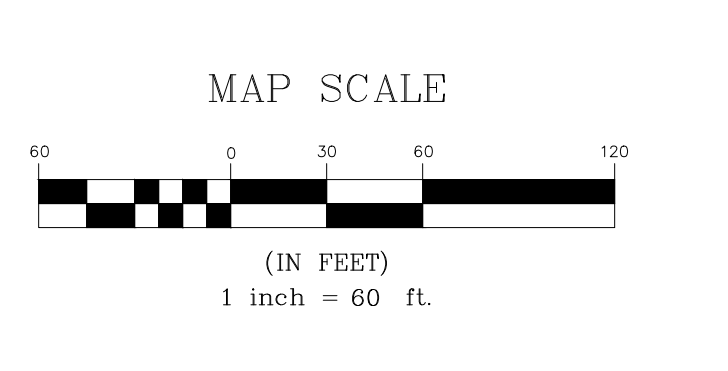
CERTIFICATION
 I HEREBY CERTIFY THAT THE LINES SHOWN ON THIS PLAN ARE THE LINES DIVIDING EXISTING OWNERSHIPS, AND THE LINES OF THE STREETS AND WAYS SHOWN ARE THOSE OF PUBLIC AND PRIVATE STREETS OR WAYS ALREADY ESTABLISHED, AND THAT NO NEW LINES FOR DIVISION OF EXISTING OWNERSHIP OF NEW WAYS ARE SHOWN.

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED IN ACCORDANCE WITH THE RULES AND REGULATIONS OF THE REGISTERS OF DEEDS OF THE COMMONWEALTH OF MASSACHUSETTS.

Jerome B. Watts
 JEROME B. WATTS MA PLS 40409

TOTAL AREA
 393,418± S.F. OR
 9.031± ACRES

TOTAL AREA
 61,971± S.F. OR
 1.423± ACRES



LEGEND:

ADJACENT LOT LINE	CONCRETE BOUND FOUND	REBAR FOUND	OWNER NO.
LOCUS BOUNDARY LINE	GRANITE BOUND FOUND	CONCRETE BOUND TO BE SET	1
INTERIOR LOCUS BOUNDARY LINE	REBAR FOUND		
EASEMENT LINE	CONCRETE BOUND TO BE SET		
EDGE OF WATER			
FLOOD ZONE BOUNDARY			

REVISIONS:

NO.	DATE	SUBMITTED FOR CLIENT REVIEW	SUBMITTED AS FINAL
0	10/5/2021		
1	11/2/2021		

Jerome B. Watts
 JEROME B. WATTS, PLS 40409 NOVEMBER 2, 2021

SGC ENGINEERING, LLC
 SURVEY AND MAPPING

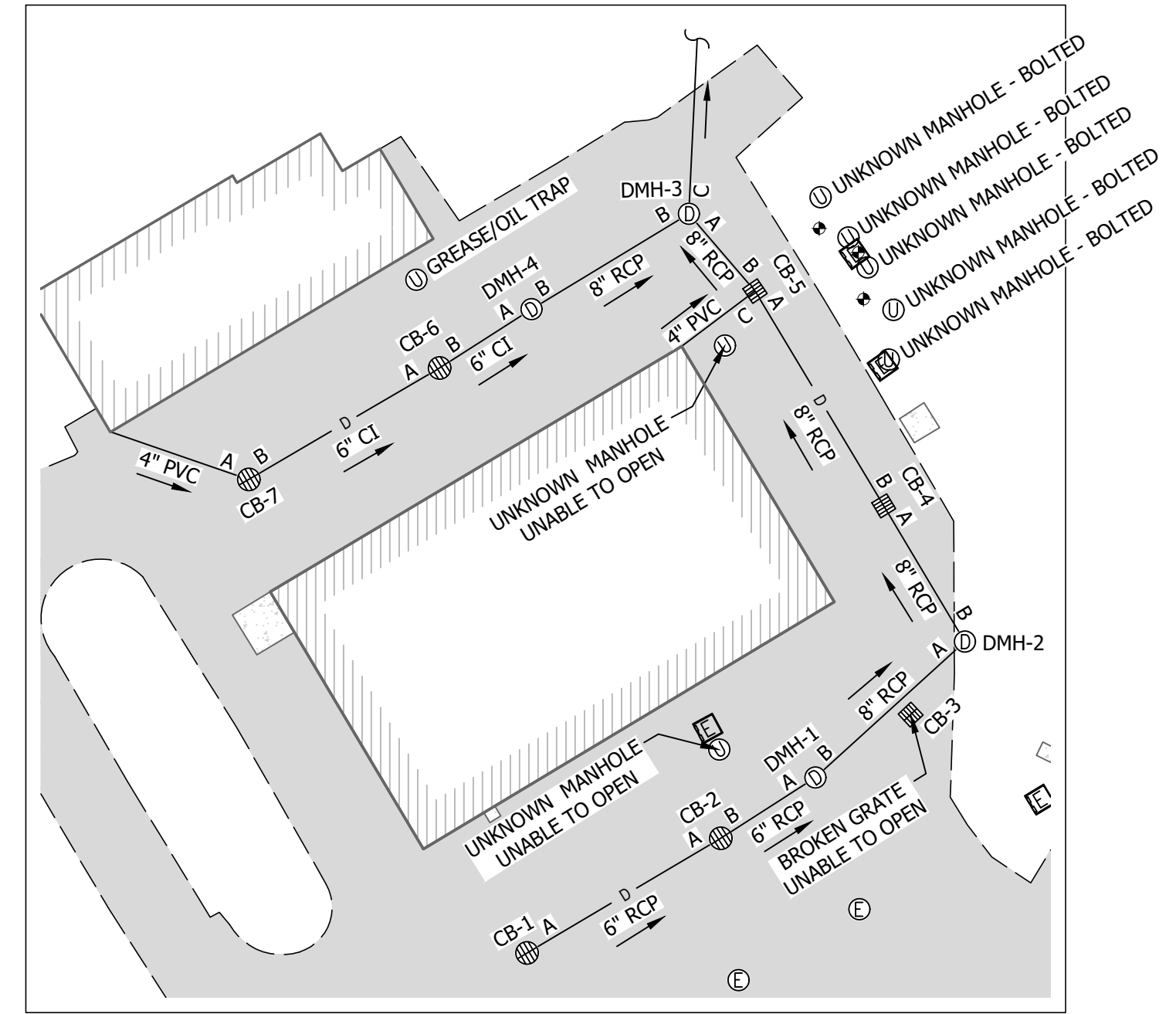
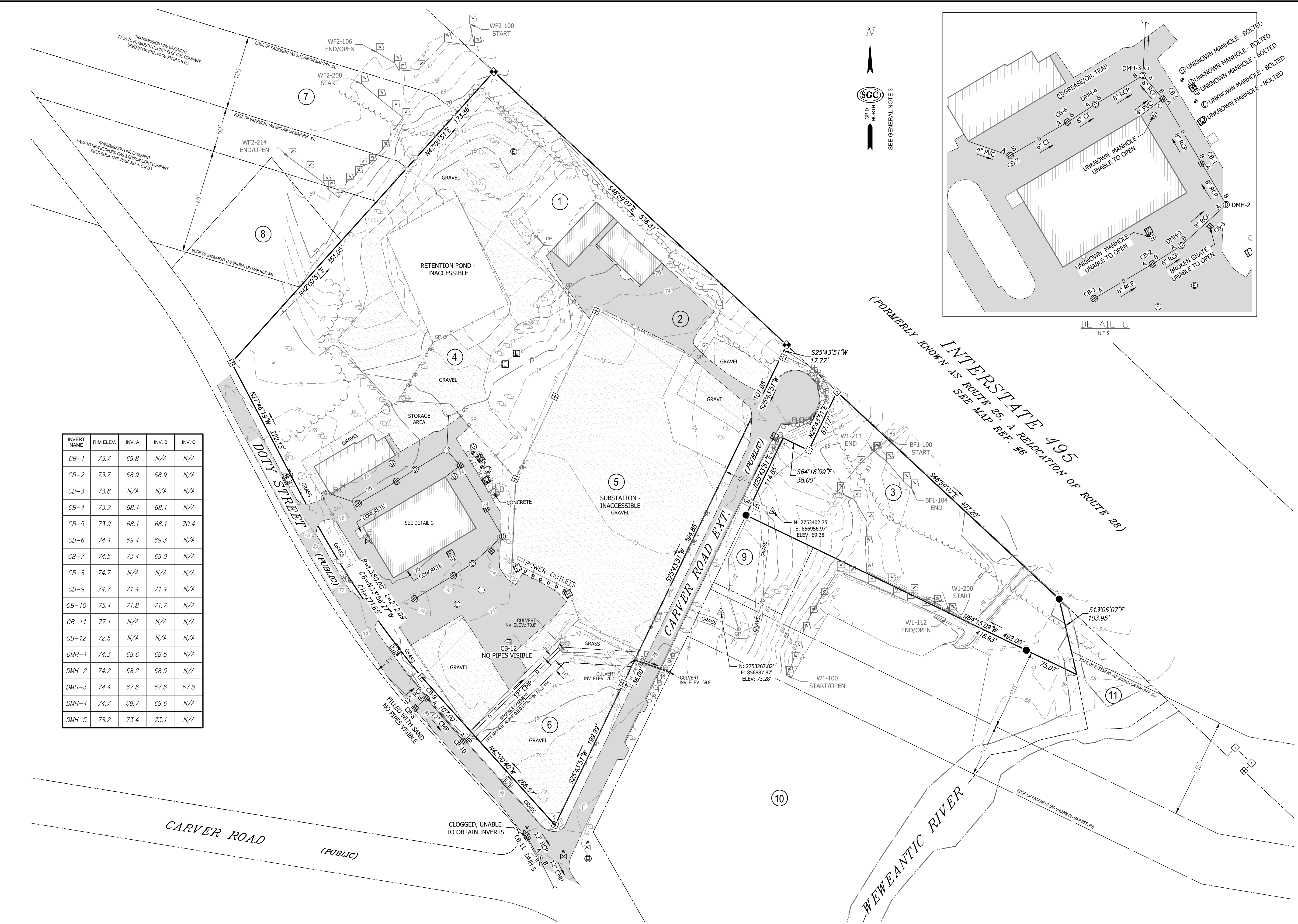
501 COUNTY ROAD
 WESTBROOK, MAINE 04092
 PHONE: 207-347-8100 // FAX: 207-347-8101
 WWW.SGCENG.COM

EVERSOURCE ENERGY

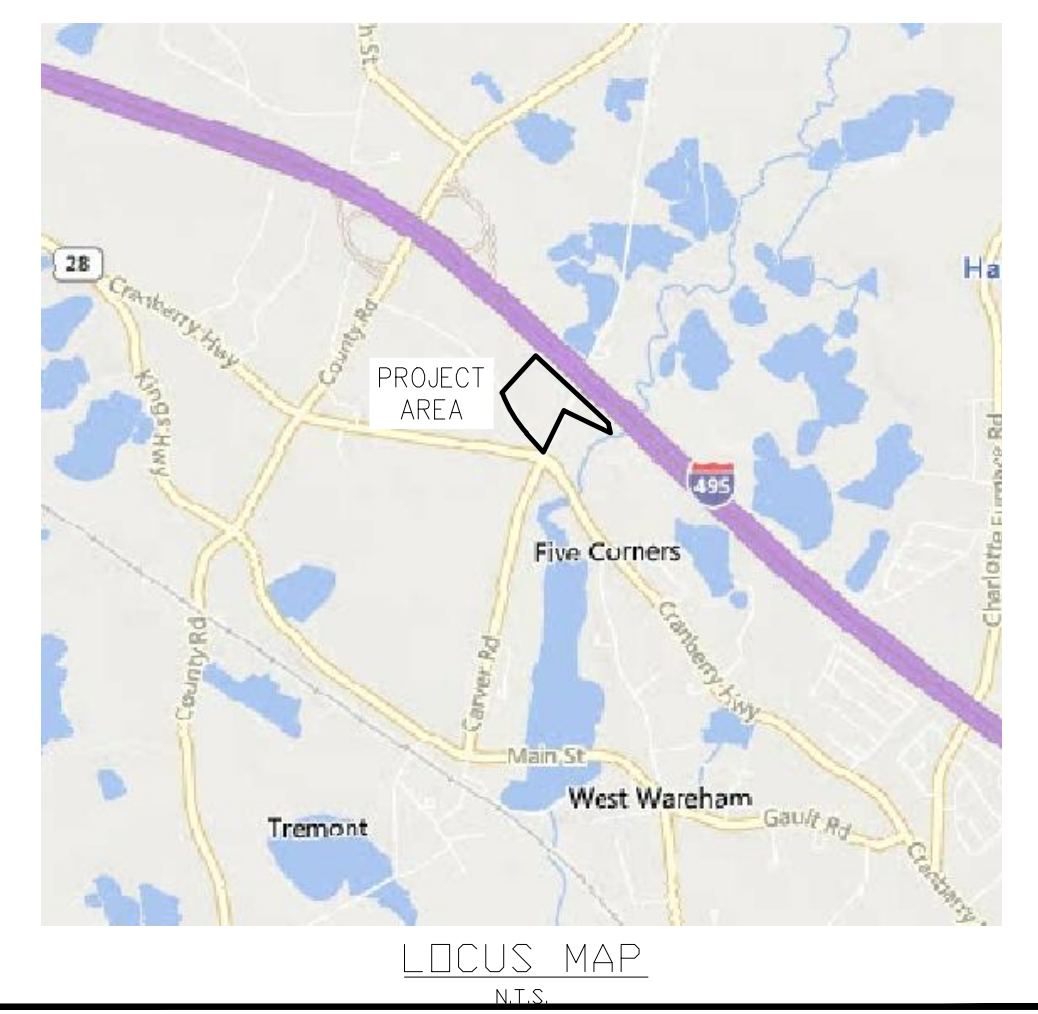
TITLE: STATION 713 BOUNDARY SURVEY DOTY STREET & CARVER ROAD EXT WAREHAM, MA

BY: JWS	CHKD: JWB	APP:	APP:
DATE: 9/7/2021	DATE: 10/5/2021	DATE:	DATE:
H-SCALE: 1" = 60'	SIZE: ARCH D	SURVEY JOB #:	
V-SCALE: N.T.S.	V.S.:	R.E.DWG.:	

R.E. PROJ. NUMBER: SHEET 1 OF 2

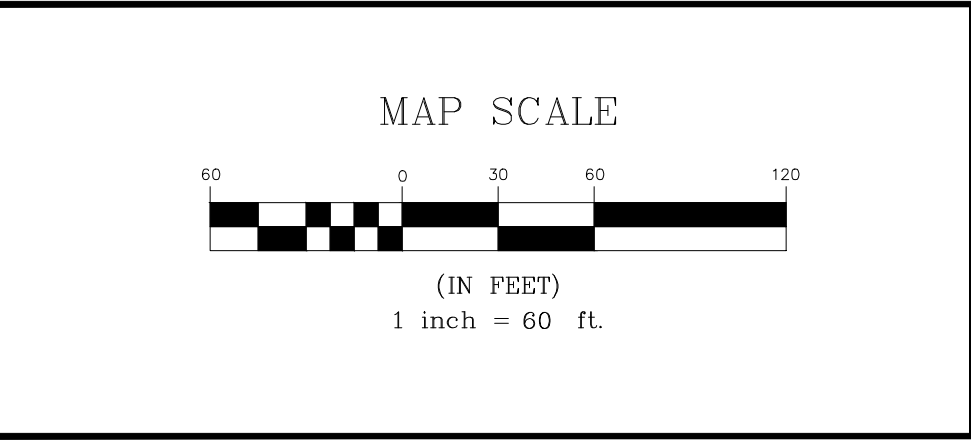


INVERT NAME	RIM ELEV.	INV. A	INV. B	INV. C
CB-1	73.7	69.8	N/A	N/A
CB-2	73.7	68.9	68.9	N/A
CB-3	73.8	N/A	N/A	N/A
CB-4	73.9	68.1	68.1	N/A
CB-5	73.9	68.1	68.1	70.4
CB-6	74.4	69.4	69.3	N/A
CB-7	74.5	73.4	69.0	N/A
CB-8	74.7	N/A	N/A	N/A
CB-9	74.7	71.4	71.4	N/A
CB-10	75.4	71.8	71.7	N/A
CB-11	77.1	N/A	N/A	N/A
CB-12	72.5	N/A	N/A	N/A
DMH-1	74.3	68.6	68.5	N/A
DMH-2	74.2	68.2	68.5	N/A
DMH-3	74.4	67.8	67.8	67.8
DMH-4	74.7	69.7	69.6	N/A
DMH-5	78.2	73.4	73.1	N/A



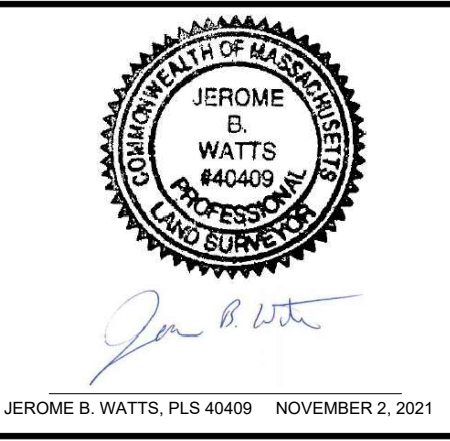
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LEGEND:

LOCUS BOUNDARY LINE	GUARDRAIL	COMMUNICATION PEDESTAL
ABUTTER BOUNDARY LINE	WETLAND DELINEATION	UNKNOWN MANHOLE TYPE
CHAINLINK FENCE	TREELINE	ELECTRIC MANHOLE
MAJOR CONTOUR	STORM DRAIN LINE	DRAIN MANHOLE
MINOR CONTOUR	BOLLARD	ELECTRIC METER
EDGE OF PAVEMENT	DECIDUOUS TREE	ELECTRIC HAND HOLE
EDGE OF CONCRETE	UTILITY POLE	POWER OUTLET
EDGE OF GRAVEL	GUY POLE	PVC CASING
RIP RAP	GUY WIRE	WATER VALVE
BUILDING	SQUARE CATCH BASIN	
	ROUND CATCH BASIN	
	SIGN	
	GATE POST	
	WETLAND FLAG	



REVISIONS:

NO.	DATE	DESCRIPTION
0	10/5/2021	SUBMITTED FOR CLIENT REVIEW
1	11/2/2021	SUBMITTED AS FINAL

SGC ENGINEERING, LLC
SURVEY AND MAPPING

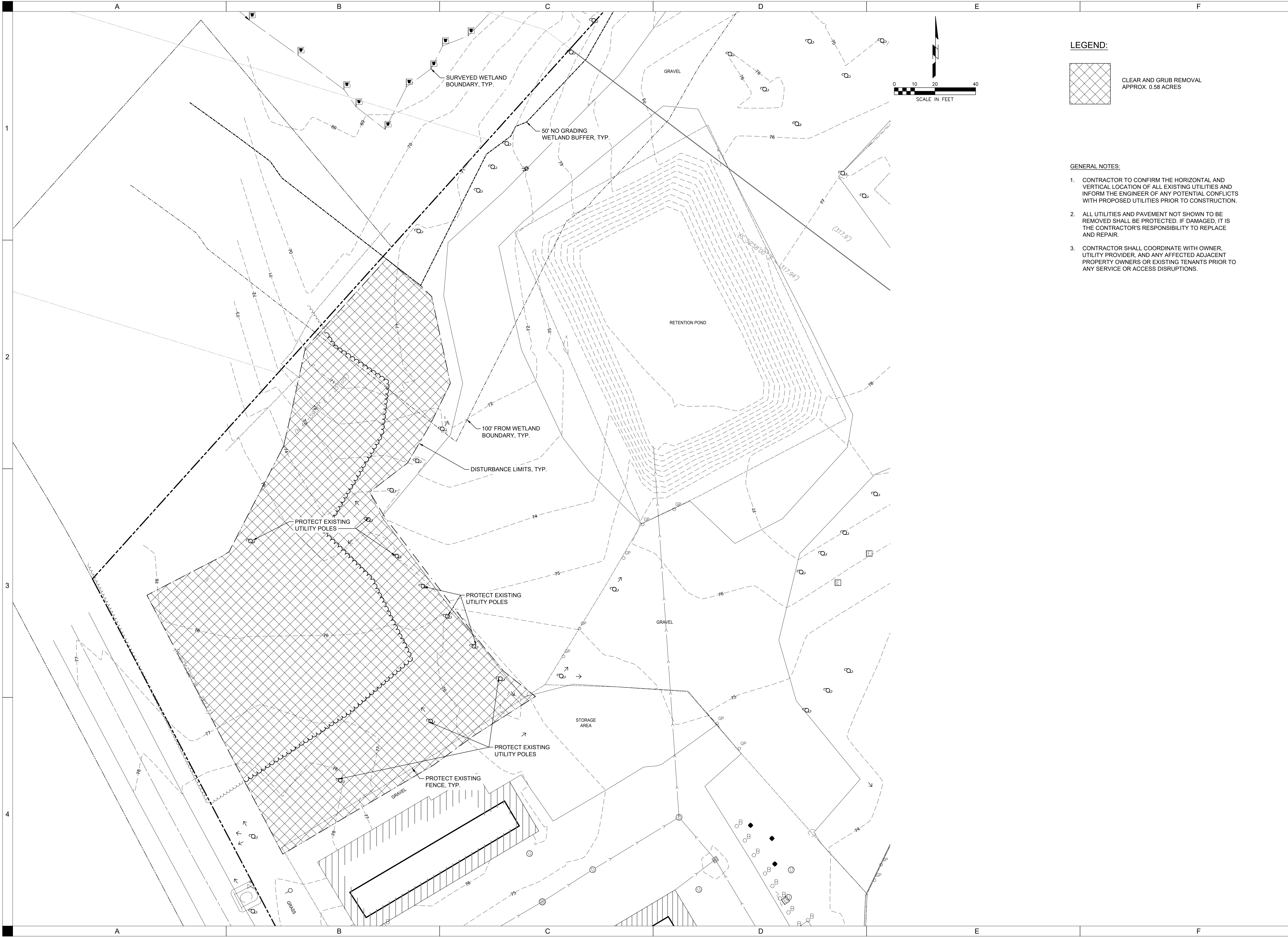
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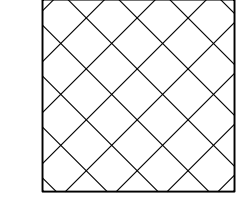
TITLE: STATION 713
EXISTING CONDITIONS PLAN
DOTY STREET & CARVER ROAD EXT
WAREHAM, MA

BY: TES	CHKD: JBW	APP:
DATE: 8/19/2021	DATE: 10/5/2021	DATE:
H-SCALE: 1" = 60'	SIZE: ARCH D	SURVEY JOB #:
V-SCALE: N.T.S.	V.S.:	R.E.DWG.:
R.E. PROJ. NUMBER:		SHEET 2 OF 2

P:\Projects\211970\Drawings\CD01\CD01 Existing Conditions & Demolition Plan.dwg
Plotted: 7/26/2022 11:41:23 AM



LEGEND:



CLEAR AND GRUB REMOVAL
APPROX. 0.58 ACRES

GENERAL NOTES:

1. CONTRACTOR TO CONFIRM THE HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES AND INFORM THE ENGINEER OF ANY POTENTIAL CONFLICTS WITH PROPOSED UTILITIES PRIOR TO CONSTRUCTION.
2. ALL UTILITIES AND PAVEMENT NOT SHOWN TO BE REMOVED SHALL BE PROTECTED. IF DAMAGED, IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPLACE AND REPAIR.
3. CONTRACTOR SHALL COORDINATE WITH OWNER, UTILITY PROVIDER, AND ANY AFFECTED ADJACENT PROPERTY OWNERS OR EXISTING TENANTS PRIOR TO ANY SERVICE OR ACCESS DISRUPTIONS.

SHIVEHATTERY
ARCHITECTURE+ENGINEERING
440 North Wells Street, Suite 270 | Chicago, Illinois 60654
312.324.5500 | www.shivehattery.com
Iowa | Illinois | Indiana | Nebraska



JEFF RATH, PE 61591 07/05/2022

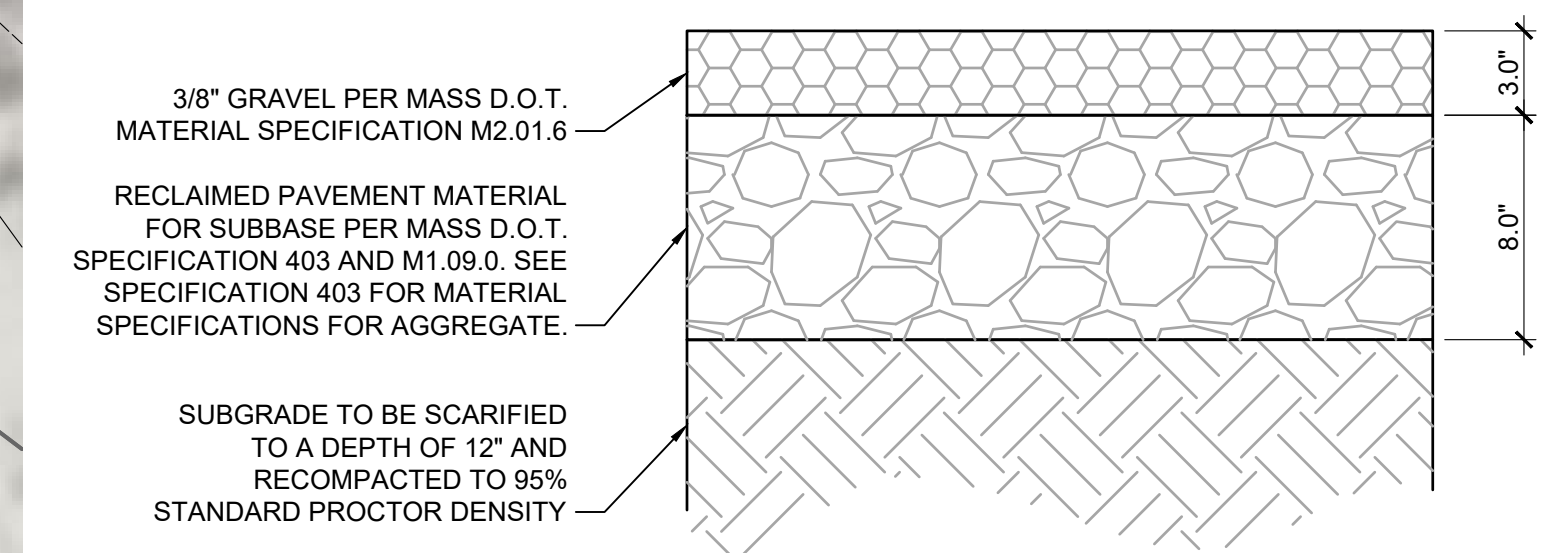
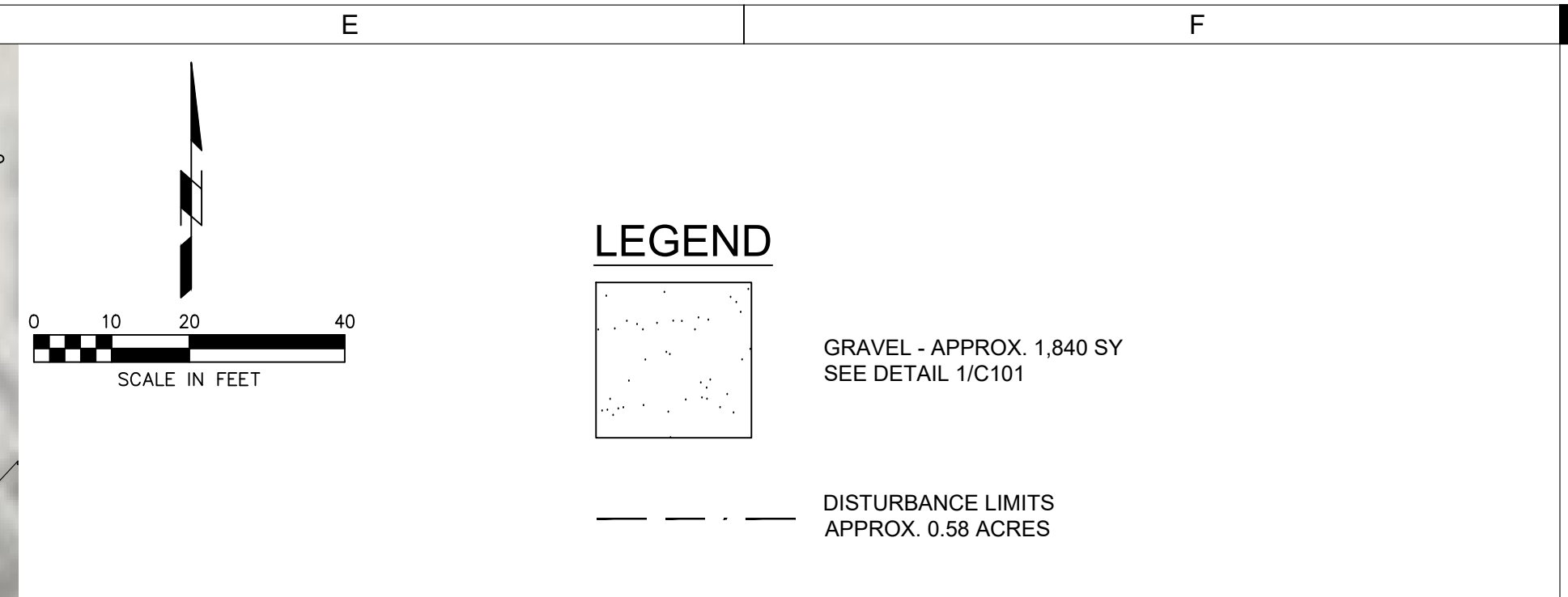
**EVERSOURCE WAREHAM
VEHICLE STORAGE LOT**
EVERSOURCE ENERGY
DOTY STREET, WAREHAM, MA 02576

**PRELIMINARY
- NOT FOR
CONSTRUCTION**

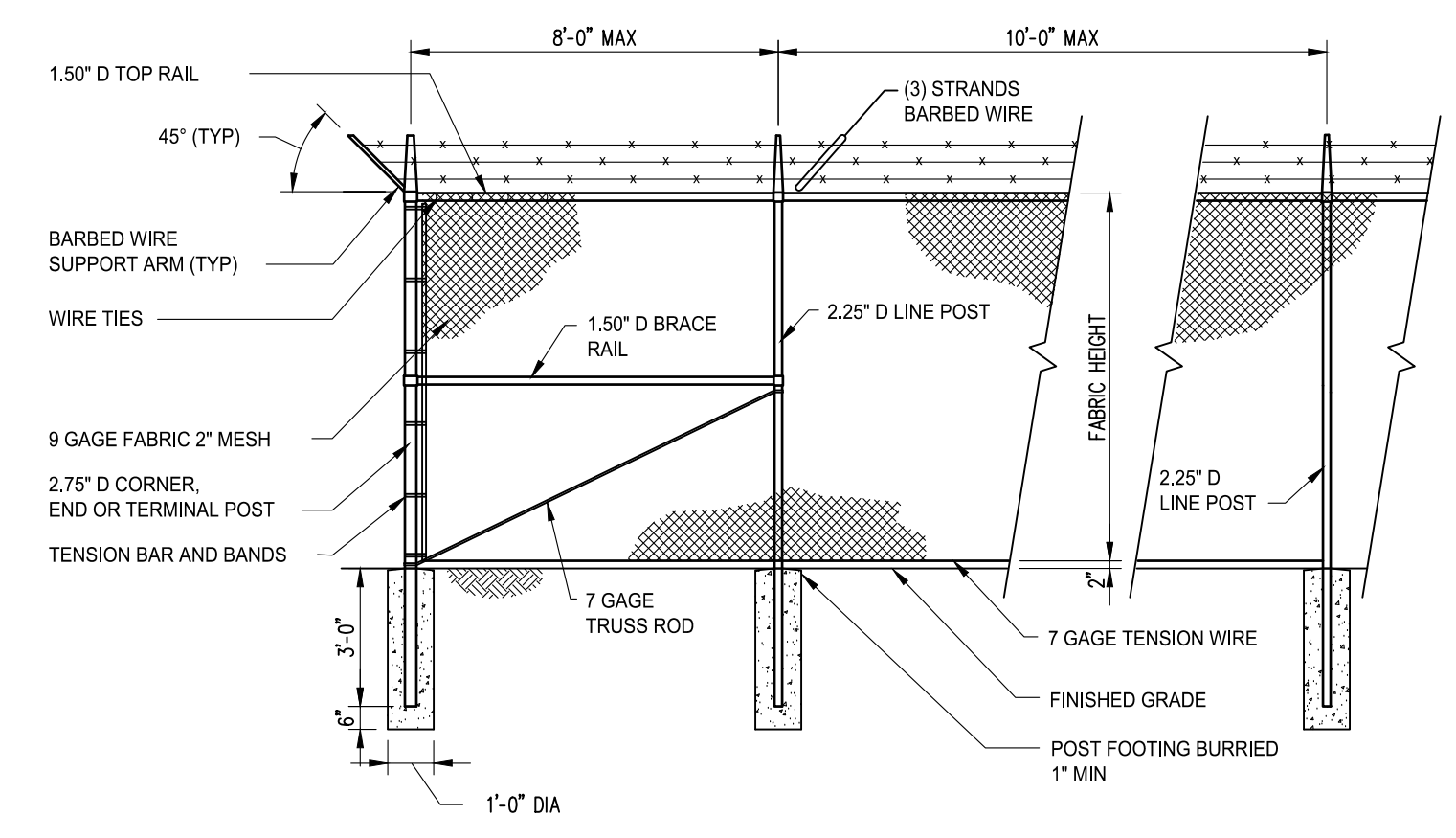
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APPROVED: JMR
ISSUED FOR: PERMIT RESUBMITTAL
DATE: 07/05/2022
PROJECT NO: 211970
FIELD BOOK: ---
CLIENT NO: ---

**EXISTING
CONDITIONS &
DEMOLITION**

CD01

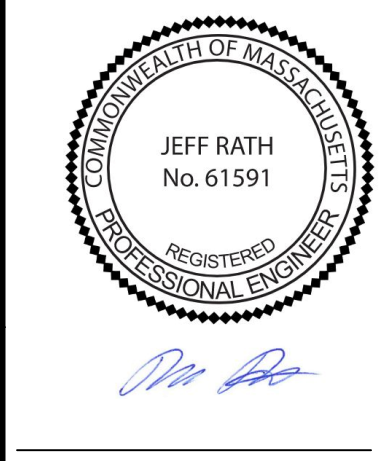


1 GRAVEL DRIVE
NOT TO SCALE



- NOTES:
1. CONTRACTOR TO MATCH EXISTING FENCE HEIGHT.
 2. TRUSS ROAD AND BRACE RAIL REQUIRED ONLY FOR FENCE HEIGHTS EQUAL TO OR GREAT THAN 8 FEET.

2 FENCE FABRIC AND POST DETAIL
NOT TO SCALE



JEFF RATH, PE 61591 07/05/2022

PRELIMINARY
- NOT FOR
CONSTRUCTION

DRAWN: EAW
APPROVED: JMR
ISSUED FOR: PERMIT RESUBMITTAL
DATE: 07/05/2022
PROJECT NO: 211970
FIELD BOOK: ---
CLIENT NO: ---

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A

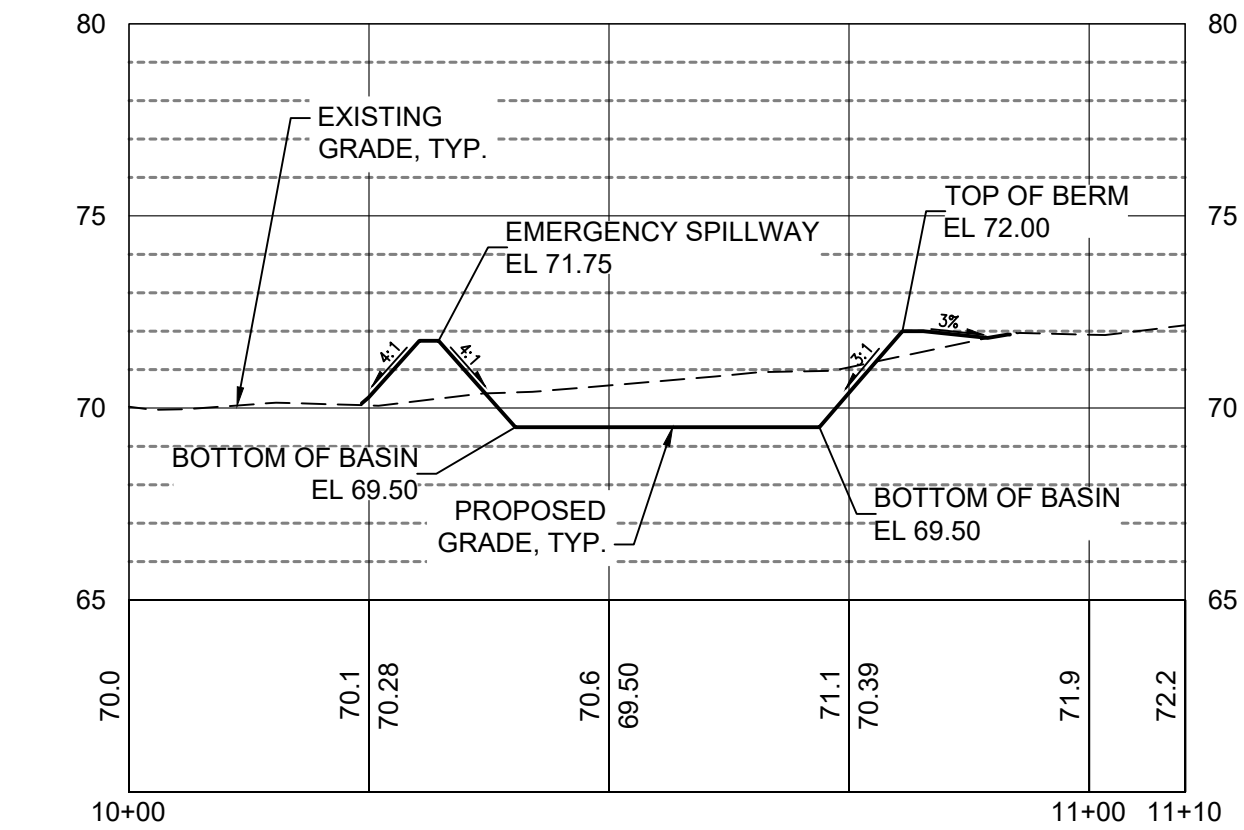
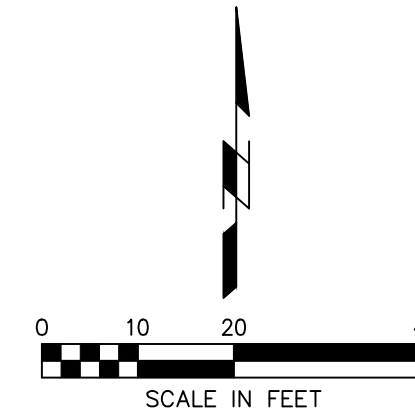
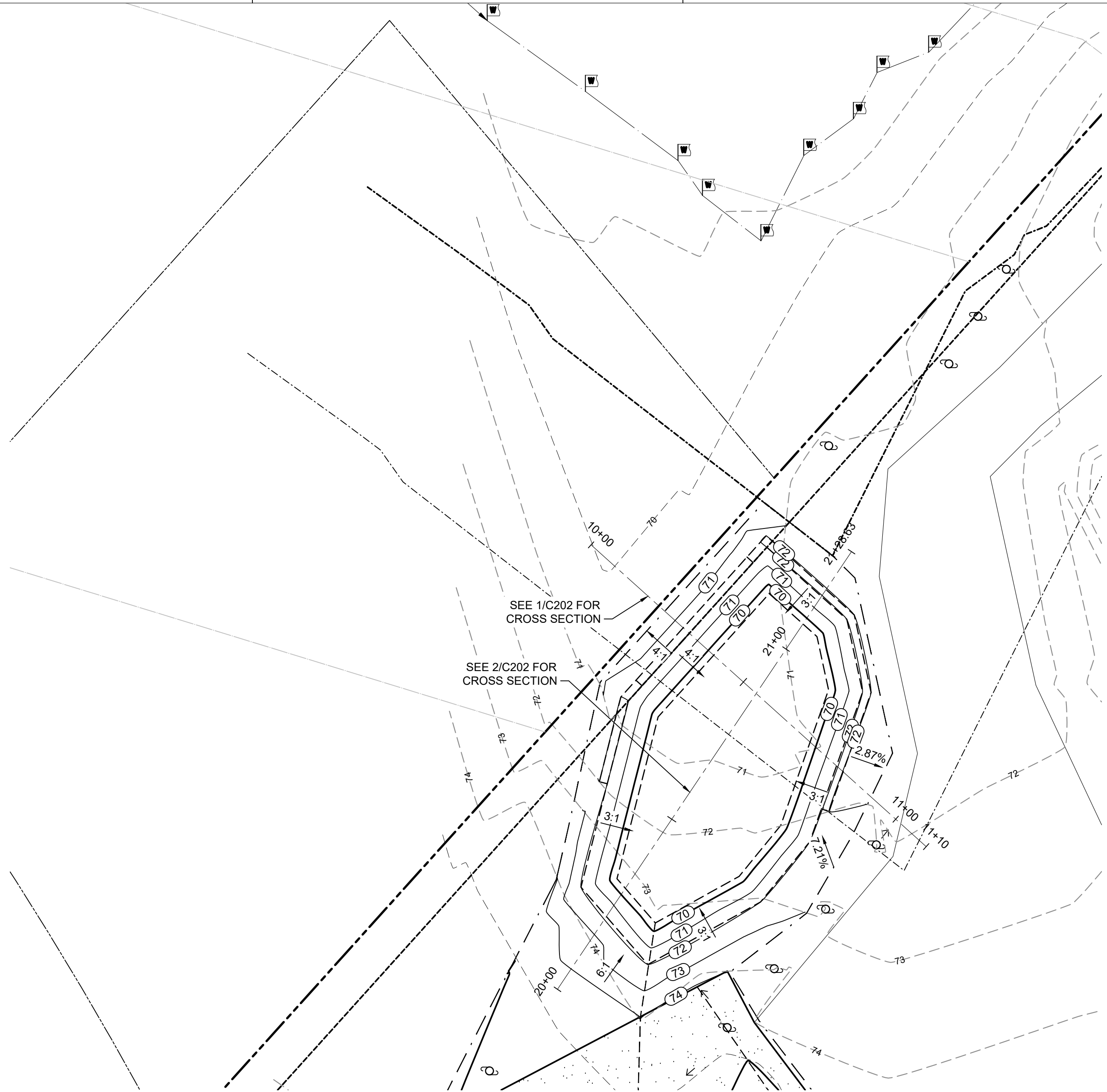
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C

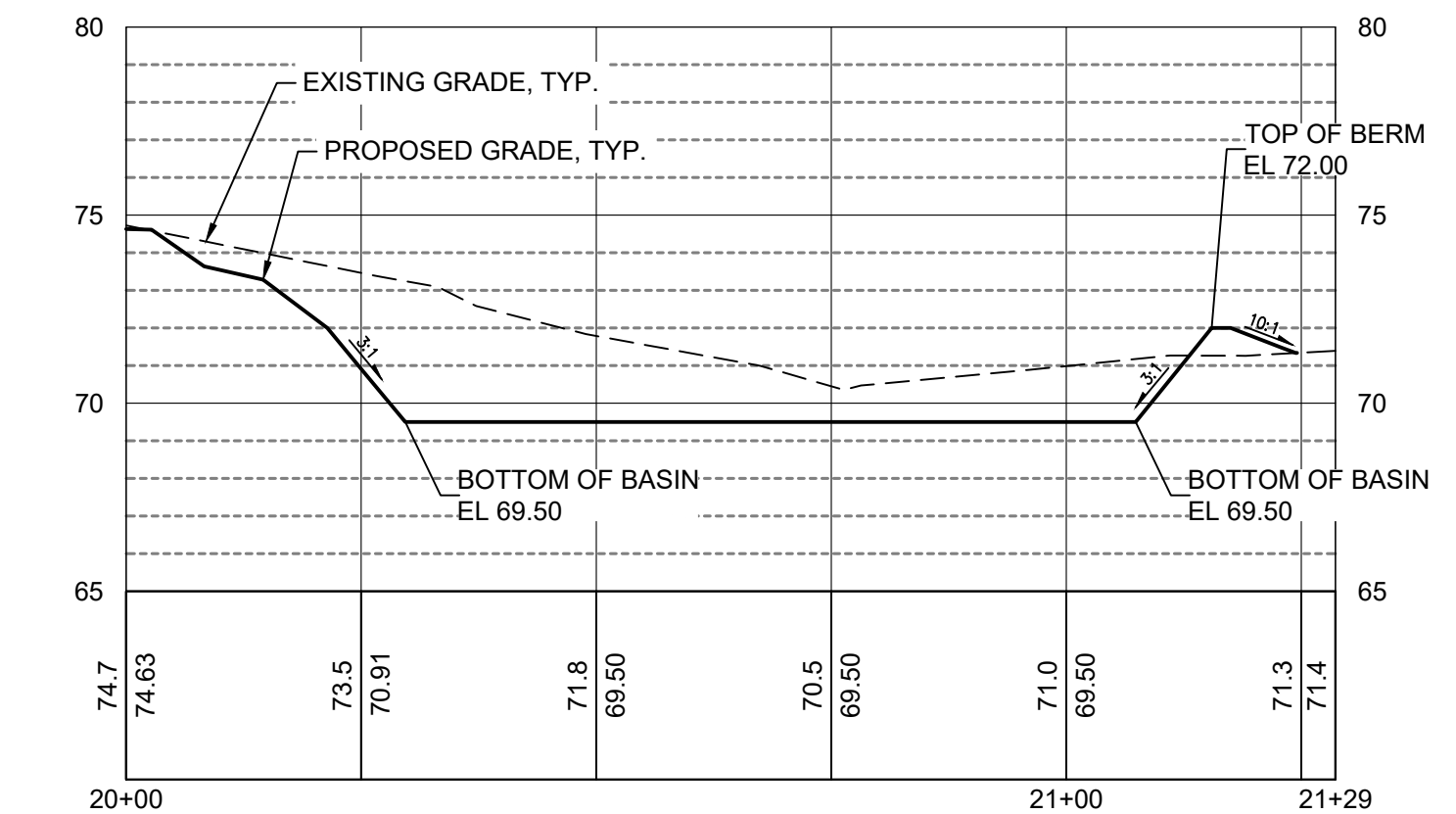
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E

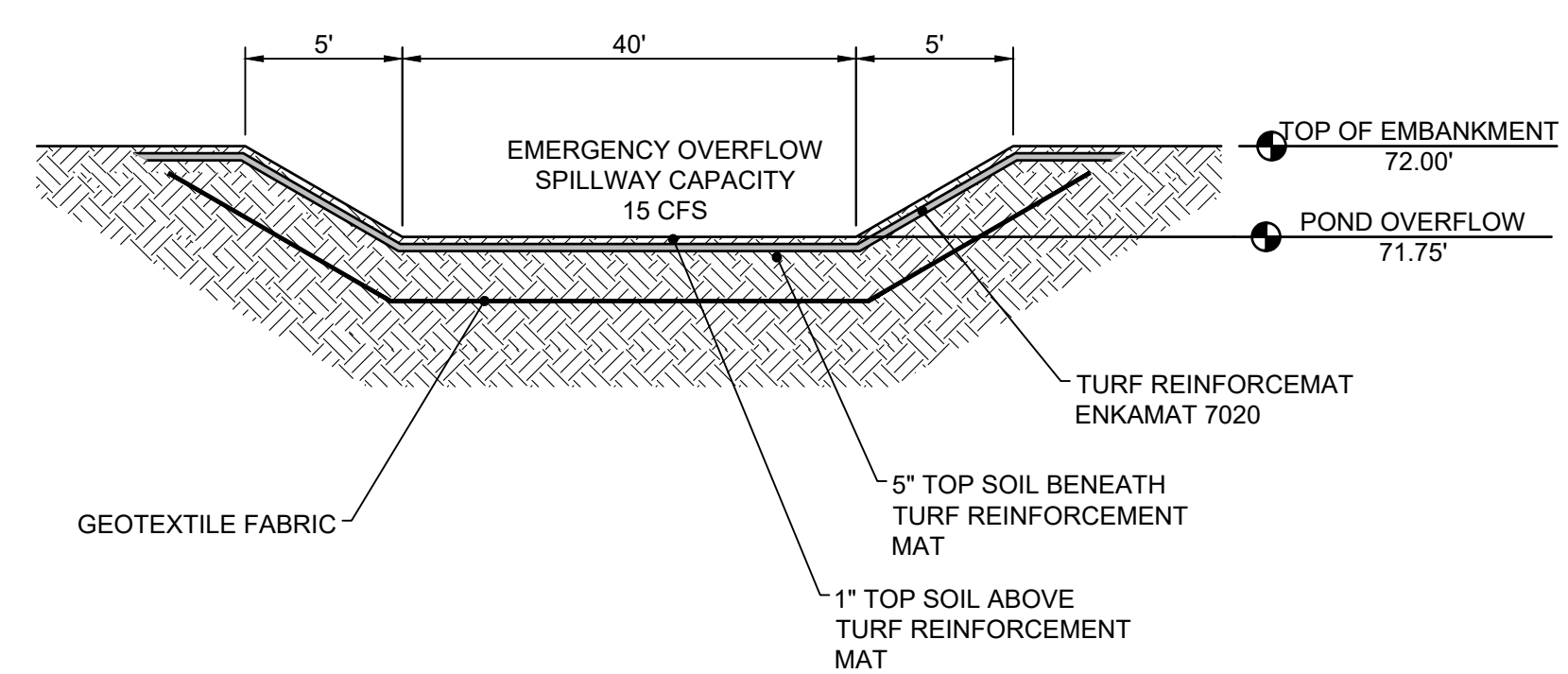
F



1 BASIN CROSS SECTION (NORTHWEST - SOUTHEAST)
H: 1" = 20' V: 1" = 5'

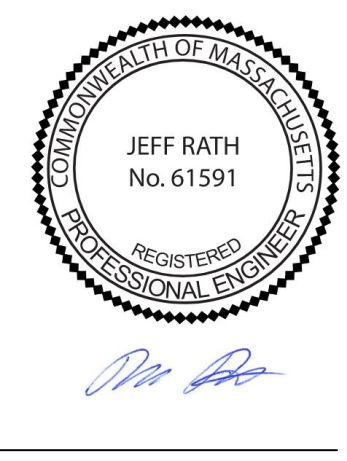


2 BASIN CROSS SECTION (SOUTHWEST - NORTHEAST)
H: 1" = 20' V: 1" = 5'



3 EMERGENCY OVERFLOW SPILLWAY DETAIL
NOT TO SCALE

SHIVEHATTERY
ARCHITECTURE + ENGINEERING
440 North Wells Street, Suite 270 | Chicago, Illinois 60654
312.324.5500 | www.shivehattery.com
Iowa | Illinois | Indiana | Nebraska



JEFF RATH, PE 61591 07/05/2022

**EVERSOURCE WAREHAM
VEHICLE STORAGE LOT**
EVERSOURCE ENERGY
DOTY STREET, WAREHAM, MA 02576

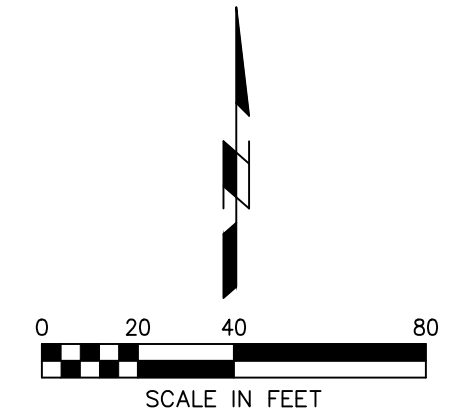
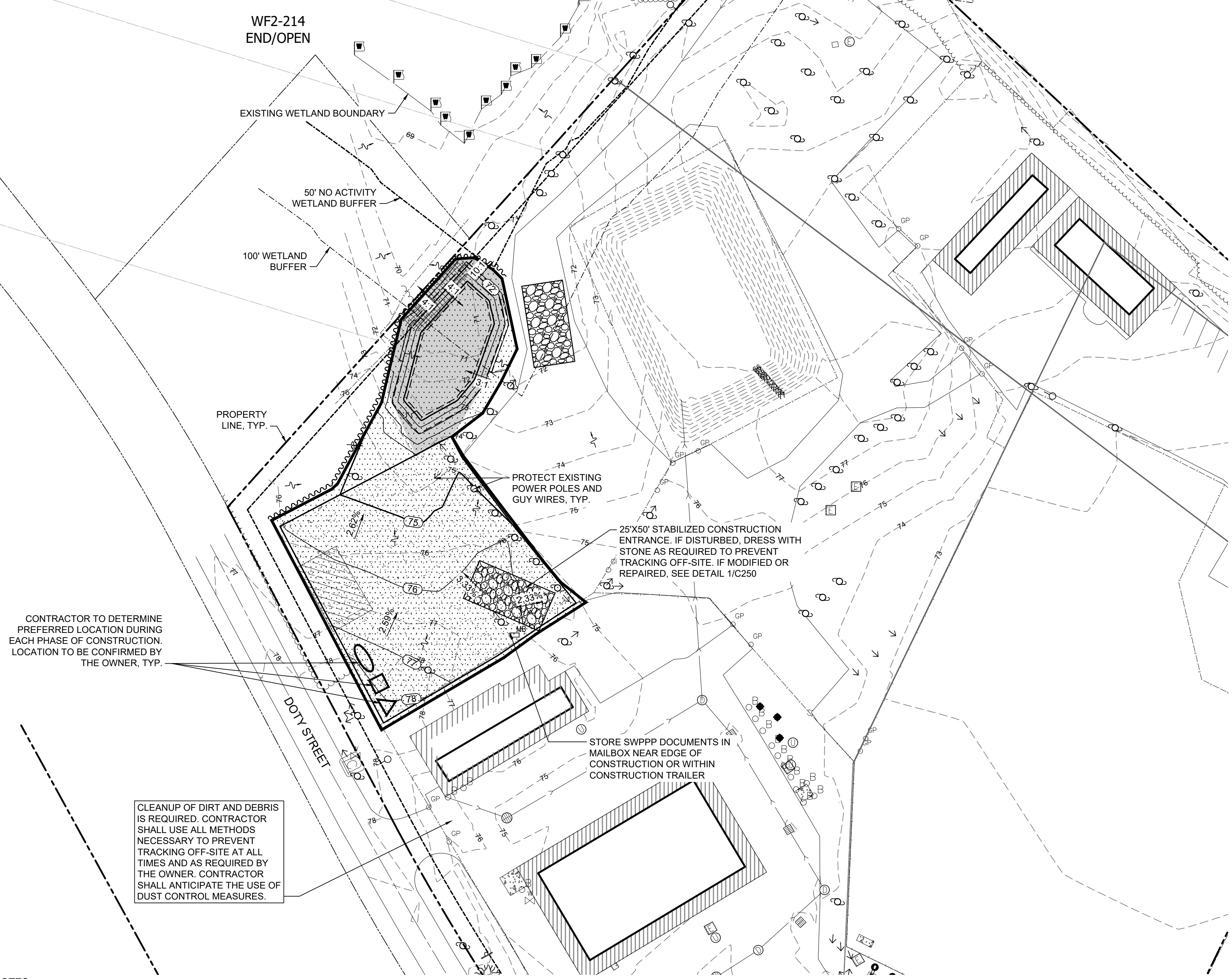
**PRELIMINARY
- NOT FOR
CONSTRUCTION**

DRAWN: EAW
APPROVED: JMR
ISSUED FOR: PERMIT RESUBMITTAL
DATE: 07/05/2022
PROJECT NO: 7211970
FIELD BOOK: --
CLIENT NO: --

**BASIN CROSS
SECTIONS**

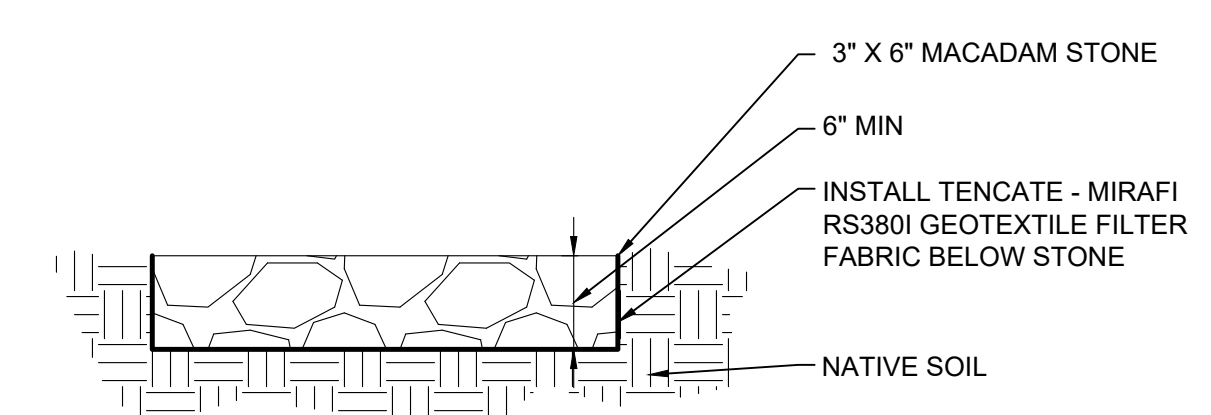
C202

P:\Projects\2021\7211970\Wareham\Drawings\C202.dwg
Printed: 7/5/2022 11:42:32 AM



- LEGEND**
- PROJECT SITE/DISTURBED AREA
 - SILT FENCE
 - FINISHED GRADE
 - DIRECTION OF DRAINAGE
 - TEMPORARY ROLLED EROSION CONTROL PRODUCT
 - TURF REINFORCEMENT MAT
 - STABILIZED CONSTRUCTION ENTRANCE/EXIT
 - CONTRACTOR STAGING AREA FOR PORTABLE RESTROOM FACILITIES, TEMPORARY FUEL TANKS, WASTE CONTAINERS AND OTHER HAZARDOUS CHEMICALS. RELOCATE AS REQUIRED FOR CONSTRUCTION.
 - CONCRETE, PAINT, AND GROUT WASHOUT AREA PER SUDAS SECT 11.050. CONTRACTOR TO HAUL OFF WASTE MATERIAL. SUGGESTED LOCATION. RELOCATE AS REQUIRED FOR CONSTRUCTION.
 - SPILL KIT TO BE INSTALLED AND RELOCATED AS REQUIRED FOR CONSTRUCTION
 - PORTABLE RESTROOM FACILITY LOCATION
 - SWPPP DOCUMENT LOCATION

THE EROSION AND SEDIMENT CONTROLS IDENTIFIED ON THIS DRAWING ARE TO BE INSTALLED AS CONSTRUCTION ACTIVITIES ALLOW. ALL TEMPORARY CONTROLS TO REMAIN UNTIL THE SITE HAS BEEN STABILIZED AND APPROVED BY THE OWNER'S REPRESENTATIVE.

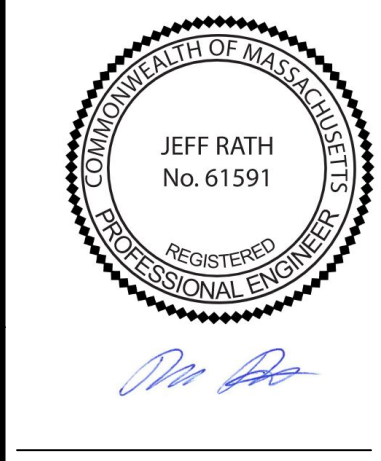


STABILIZED CONSTRUCTION ENTRANCE/ CONTRACTOR STAGING AND LAYDOWN AREA (AS REQUIRED)

1 NOT TO SCALE

EROSION AND SEDIMENT CONTROL NOTES

1. EROSION/ SEDIMENTATION CONTROL MEASURES SHOULD BE INSTALLED BEFORE EARTH DISTURBING ACTIVITIES BEGIN AND ARE REQUIRED REGARDLESS OF THE TIME OF YEAR. THIS PLAN AND ITS ASSOCIATED REQUIREMENTS FOR THE PERMIT MUST BE IMPLEMENTED DURING WINTER MONTHS AS WELL. ADDITIONAL INSTALLATION/REMOVAL OF STABILIZATION CONTROLS ASSOCIATED WITH SEEDING OUTSIDE THE SEEDING WINDOW ARE INCIDENTAL TO THE PROJECT.
2. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTROL EROSION/SEDIMENT ON THE SITE AT ALL TIMES. THE CONTROL MEASURES IDENTIFIED ON THE PLAN ARE A MINIMUM. THE CONTRACTOR SHALL PROVIDE ADDITIONAL EROSION/SEDIMENT CONTROL MEASURES AS NECESSARY AND BY CONSTRUCTION PHASE, TO FULFILL THIS REQUIREMENT.
3. CONTRACTOR IS REQUIRED TO MAINTAIN SWPPP DOCUMENTATION.
4. EXCEPT AS PRECLUDED BY SNOW COVER, THE CONTRACTOR IS REQUIRED TO USE STABILIZATION CONTROLS ON ALL DISTURBED AREAS OF THE SITE REGARDLESS OF THE TIME PERIOD BEFORE THEY WILL BE DISTURBED AGAIN. IN THE EVENT THAT CONSTRUCTION ACTIVITY WITHIN A DISTURBED AREA WILL NOT OCCUR FOR A PERIOD OF 14 OR MORE CALENDAR DAYS, THE CONTRACTOR IS REQUIRED TO INSTALL STABILIZATION MEASURES IMMEDIATELY AFTER CONSTRUCTION ACTIVITY CEASED IN THAT AREA.
5. THE CONTRACTOR SHALL USE CONTROL MEASURES AS REQUIRED TO KEEP SOILS FROM LEAVING THE SITE.
6. CONTRACTOR SHALL IMPLEMENT SITE SPECIFIC BEST MANAGEMENT PRACTICES (BMPs) AS SHOWN AND REQUIRED BY THE SWPPP. ADDITIONAL BEST MANAGEMENT PRACTICES SHALL BE IMPLEMENTED BY THE CONTRACTOR AS DICTATED BY SITE CONDITIONS OR THE PROJECT GOVERNING AUTHORITIES AT NO ADDITIONAL COST TO THE OWNER THROUGHOUT ALL PHASES OF CONSTRUCTION.
7. IF AFTER REPEATED FAILURE ON THE PART OF THE CONTRACTOR TO PROPERLY CONTROL SOIL EROSION, SEDIMENT AND/OR POLLUTION FROM THE PROJECT SITE, THE GOVERNING AUTHORITIES RESERVE THE RIGHT TO EFFECT NECESSARY CORRECTIVE MEASURES AND CHARGE ANY COSTS TO THE CONTRACTOR.
8. ALL BMPs AND CONTROLS SHALL CONFORM TO THE APPLICABLE FEDERAL, STATE, OR LOCAL REQUIREMENTS, STANDARDS, AND SPECIFICATIONS OR MANUAL OF PRACTICE.
9. ALL BMPs AND CONTROLS INSTALLED ON GREEN INFRASTRUCTURE SHALL REMAIN UNTIL STABILIZATION IS APPROVED BY THE OWNER'S REPRESENTATIVE.
10. IN THE EVENT THAT SOILS LEAVE THE SITE, CLEANUP OF ALL SURROUNDING ROADS, DRIVES, AND PARKING LOTS SHALL BE PERFORMED ON A DAILY BASIS AT A MINIMUM AND UPON REQUEST BY OWNER'S REPRESENTATIVE AT NO ADDITIONAL COST. PAVEMENT IS TO BE SCRAPPED OF DEBRIS AND MUD AND BROOMED CLEAN. MUD TRACKS ARE TO BE REMOVED AS THEY ARE CREATED.
11. IF DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIALS ARE DEPOSITED IN THE FLOW LINE OF GUTTERS, DRAINAGE STRUCTURES, OR DITCHES SUCH THAT THE NATURAL FLOW LINE OF WATER IS OBSTRUCTED, THIS LOOSE MATERIAL SHALL BE REMOVED.
12. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING ANY EXISTING STORM DRAINAGE SYSTEMS BY THE USE OF INLET PROTECTION OR OTHER APPROVED FUNCTIONAL METHODS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING SEDIMENT RESULTING FROM CONSTRUCTION ACTIVITIES ASSOCIATED WITH THIS PROJECT.
13. CONSTRUCTION ACCESS POINTS TO THE SITE SHALL BE PROTECTED IN SUCH A WAY AS TO PREVENT TRACKING OF MUD OR SOIL ONTO PUBLIC THOROUGHFARES. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHTS-OF-WAY MUST BE REMOVED IMMEDIATELY BY THE CONTRACTOR.
14. MAINTAIN SILT FENCING AT ALL TIMES IN AN UPRIGHT POSITION. CLEAN SILT FROM FENCING/FILTER SOCKS ON A REGULAR BASIS AS PER THE STANDARD SPECIFICATIONS. SILT FENCES MUST BE CLEANED OUT WHEN THEY ARE 50% FULL. FILTER SOCKS MUST BE CLEANED OUT WHEN THEY ARE 93% FULL.
15. CONTRACTOR TO LOCATE/RELOCATE FILTER SOCKS AS NECESSARY THROUGHOUT THE PROJECT TO CONTROL EROSION/SEDIMENT.
16. REMOVE ALL TEMPORARY EROSION/SEDIMENTATION CONTROLS NOT CALLED OUT TO REMAIN AFTER SITE HAS BEEN STABILIZED AND APPROVED BY THE OWNER'S REPRESENTATIVE.
17. CONTRACTOR TO USE EXTREME CAUTION WHILE INSTALLING SILT FENCE OR OTHER EROSION CONTROL DEVICES SO AS NOT TO DAMAGE UNDERGROUND UTILITIES.
18. WHERE WATER IS PUMPED FROM EXCAVATIONS ON SITE, PROVISIONS SHALL BE MADE TO REMOVE SEDIMENT FROM THE WATER BEFORE IT IS RELEASED INTO THE STORM SEWER SYSTEM. METHODS INCLUDE: DEWATERING BAGS, ADDING FLOCCULANTS TO SILTY WATER AND PLACING A FILTER FABRIC BARRIER AROUND THE PUMP INLET. THE COMBINATION OF THESE METHODS HELPS TO REMOVE SEDIMENT FROM THE WATER. THE MOST COMMON METHOD INCLUDES PLACING CHITOSAN GEOTEXTILE TREATMENT BAGS WITHIN THE DEWATERING BAG AND INSTALLING AN ADDITIONAL INTAKE PROTECTION BAG AT THE NEAREST DOWNSTREAM INTAKE.
19. WHERE WATER IS RELEASED FROM A DEWATERING SYSTEM, PRECAUTIONS SHALL BE TAKEN TO ENSURE THAT EROSION GULLIES DO NOT FORM. ONE METHOD IS TO PIPE THE WATER DIRECTLY INTO A STORM SEWER STRUCTURE. WATER MUST BE FILTERED THROUGH AN APPROPRIATE FILTER DEVICE BEFORE DISCHARGING.
20. CONCRETE SLURRY AND DUST FROM SAWCUTTING ACTIVITIES IS PROHIBITED FROM ENTERING THE STORM SEWER SYSTEM. ALL STORM INTAKES LOCATED NEAR SAWCUTTING ACTIVITIES ARE REQUIRED TO BE PROTECTED. SAWCUT SLURRY AND DUST MUST BE CONTAINED, CLEANED UP, AND DISPOSED OF OFF-SITE. A TEMPORARY 8" FILTER SOCK IS REQUIRED.
21. SANITARY WASTE DISPOSAL: ALL LOCATIONS OF PORTABLE RESTROOM FACILITIES MUST BE IDENTIFIED ON THE PLAN. IN THE EVENT THAT PORTABLE RESTROOM FACILITIES ARE USED ON-SITE, THE CONTRACTOR IS REQUIRED TO INSTALL AN 8" FILTER SOCK AROUND THE FACILITY TO MINIMIZE THE RADIUS OF THE AFFECTED ZONE IN THE EVENT OF A SPILL. WASTES SHALL BE COLLECTED AND DISPOSED OF IN COMPLETE COMPLIANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS. PORTABLE RESTROOM FACILITIES MUST NOT BE LOCATED NEAR DRAINAGE WAYS. RELOCATE AS REQUIRED FOR CONSTRUCTION.
22. IDENTIFICATION OF ALLOWABLE NON-STORMWATER DISCHARGES: DURING CONSTRUCTION THE NON-STORMWATER DISCHARGES, WHICH INCLUDE WATER FLUSHED FROM WATER LINES, PAVEMENT WASHING (WHERE NO SPILLS OR LEAKS HAVE OCCURRED, UNLESS THE SPILLED MATERIAL HAS BEEN CLEANED UP), VEHICLE WASHING, AND GROUNDWATER (DEWATERING), SHOULD BE DIRECTED AS MUCH AS POSSIBLE TOWARDS VEGETATED AREAS AND AWAY FROM DRAINAGE WAYS. REFER TO THE OKLAHOMA DEQ NPDES GENERAL PERMIT NO. 2 FOR ALLOWABLE NON-STORMWATER DISCHARGES.
23. POLLUTION AND SPILL PREVENTION PLANNING: POTENTIALLY HAZARDOUS MATERIALS ON THE CONSTRUCTION SITE INCLUDE FUEL, LUBRICANTS, CURING COMPOUNDS, FERTILIZERS, GREASE AND CLEANING SOLVENTS. THE CONTRACTOR STAGING AREA FOR PORTABLE RESTROOM FACILITIES, TEMPORARY FUEL TANKS, WASTE CONTAINERS AND OTHER HAZARDOUS CHEMICALS MUST BE PROTECTED BY AN 8" FILTER SOCK AT ALL TIMES. ALL REASONABLE PRECAUTIONS WILL BE TAKEN TO PREVENT SPILLS. ANY SPILLED MATERIAL WILL IMMEDIATELY BE DIRECTED AWAY FROM STORMWATER INTAKES, DETENTION BASINS, OR DRAINAGE WAYS. SPILLED MATERIALS WILL BE CLEANED AND, IF NECESSARY, SOIL REMEDIATION PRACTICES WILL BE USED. A RECORD OF SPILLS WILL BE MAINTAINED BY THE PRIME CONTRACTOR. RELOCATE AS REQUIRED FOR CONSTRUCTION.
24. CONCRETE, PAINT AND GROUT WASHOUT AREA: THE WASHOUT AREA SHOULD BE AN APPROVED CONCRETE WASHOUT CONTAINER, COLLECTION BAG, OR WASHOUT BOX. IF USING FILTER SOCKS, STACK TWO (2) TALL AND LINE WITH AN IMPERMEABLE PLASTIC LINER. CONTRACTOR TO HAUL OFF ALL WASTE MATERIAL. ALL LOCATIONS OF CONCRETE, PAINT AND GROUT WASHOUT AREAS MUST BE PROVIDED BY THE CONTRACTOR AND IDENTIFIED ON THE PLAN. THE CONTRACTOR IS REQUIRED TO INSTALL A SIGN THAT DESIGNATES THE WASHOUT AREA. RELOCATE AS REQUIRED FOR CONSTRUCTION.
25. SPILL KIT: A SPILL KIT IS REQUIRED TO BE ON-SITE AND LOCATION NOTED ON THE STORMWATER POLLUTION PREVENTION PLAN. THE SPILL KIT SHOULD BE DESIGNED TO DEAL WITH ANY HAZARDOUS MATERIALS ON-SITE. THE SPILL KIT SHALL BE A SEALED STORAGE SHED LOCATED NEAR THE CONSTRUCTION TRAILER OR FUELING AREA. THE SPILL KIT SHALL CONTAIN, BUT NOT BE LIMITED TO THE FOLLOWING ITEMS: A GARBAGE CAN, GLOVES, SAFETY GOGGLES, BROOM AND DUST PAN AND OIL ABSORBENT CLAY CHIPS OR PADS. THE SPILL KIT SHALL BE RESTOCKED AS SUPPLIES ARE USED. THE CONTRACTOR SHALL INSTALL A SIGN THAT DESIGNATES THE SPILL KIT. RELOCATE AS REQUIRED FOR CONSTRUCTION.
26. DUST CONTROL: THE CONTRACTOR SHALL IMPLEMENT DUST CONTROL MEASURES WHERE DUST IS GENERATED. FREQUENT WATERING OF THE SITE, SPRINKLED VEGETATIVE COVER, MULCH, WINDBREAKS, TILLAGE, STONE AND SPRAY-ON CHEMICAL SOIL TREATMENTS (PALLIATIVES) ARE POSSIBLE DUST CONTROL MEASURES. IF THE DUST CONTROL IS NOT ACCEPTABLE IT SHALL BE CHANGED AS DIRECTED BY THE OWNER'S REPRESENTATIVE.
27. STOCKPILED MATERIALS: CONTRACTOR TO IDENTIFY ALL LOCATIONS OF STOCKPILED MATERIALS ON THE STORMWATER POLLUTION PREVENTION PLAN. CONTRACTOR SHALL PROVIDE ALL EROSION/SEDIMENTATION CONTROLS AS REQUIRED TO CONTAIN MATERIALS ON-SITE. AT A MINIMUM, THE CONTRACTOR IS REQUIRED TO PROVIDE SILT FENCE/FILTER SOCKS AROUND STOCKPILED SOILS BEFORE STOCKPILE IS RE-SPREAD. IF STOCKPILE SOILS WILL REMAIN INACTIVE FOR 14 DAYS OR MORE, THE CONTRACTOR TO SHALL COVER OR PROVIDE TEMPORARY STABILIZATION CONTROLS.
28. SEQUENCE OF MAJOR ACTIVITIES: INCORPORATE ALL TEMPORARY STABILIZING AND PERMANENT EROSION/SEDIMENT CONTROL FEATURES AT THE EARLIEST TIME PRACTICABLE. THE CONTRACTOR SHALL AMEND THE SWPPP WHENEVER THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION OR MAINTENANCE OF A STORMWATER BMP.



PRELIMINARY - NOT FOR CONSTRUCTION

DRAWN: EAW	APPROVED: JMR
ISSUED FOR: PERMIT RESUBMITTAL	DATE: 07/02/2022
PROJECT NO: 211970	FIELD BOOK: ---
CLIENT NO: ---	

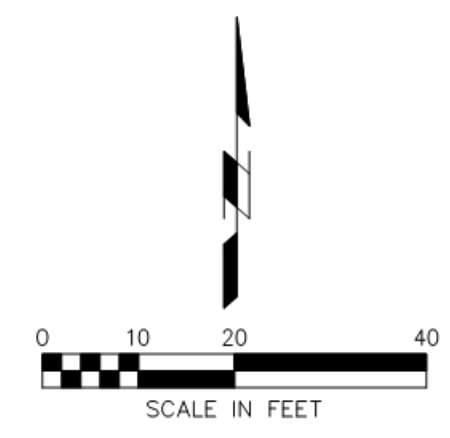
EROSION & SEDIMENT CONTROL PLAN

C250



LEGEND

- GRAVEL
- TURF SEEDING LOW GROW FESCUE
- DISTURBANCE LIMITS
- 3" SHREDDED MULCH BED
- STEEL METAL EDGING
- 3" DEPTH RIVER ROCK BED



PLANT SCHEDULE

TREES	CODE	QTY	BOTANICAL NAME	COMMON NAME	ROOT	SIZE	
	ACE ABZ	2	Acer x freemanii 'Jeffersred' TM	Autumn Blaze Freeman Maple	B & B	2" Cal	
	QUE BIC	5	Quercus bicolor	Swamp White Oak	B & B	2" Cal	
SHRUBS	CODE	QTY	BOTANICAL NAME	COMMON NAME	ROOT	SIZE	SPACING
	KAL SAR	13	Kalmia latifolia 'Sarah'	Sarah Mountain Laurel	3 gal	24 - 30" Ht	48" o.c.
	VIB MUF	27	Viburnum dentatum 'Blue Muffin'	Blue Muffin Arrowwood Viburnum	3 gal	30 - 36" Ht.	48" o.c.
ORNAMENTAL GRASS	CODE	QTY	BOTANICAL NAME	COMMON NAME	ROOT	SIZE	SPACING
	PAN NOR	29	Panicum virgatum 'Northwind'	Northwind Switch Grass	2 gal	24 - 30" Ht	36" o.c.

PLANTING AND SITE RESTORATION NOTES

- PROTECT ALL EXISTING AND NEW STRUCTURES AND UTILITY SERVICES DURING INSTALLATION OF ALL PLANT MATERIAL. IF CONFLICTS EXIST BETWEEN PROPOSED PLANTING LOCATIONS AND OTHER STRUCTURES AND UTILITIES, CONTRACTOR SHALL COORDINATE PLANTING ADJUSTMENTS WITH LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- ALL PLANTING WORK SHALL BE COORDINATED WITH THE WORK OF OTHER TRADES ON SITE.
- LANDSCAPE CONTRACTOR IS REQUIRED TO MAINTAIN POSITIVE DRAINAGE ON THE SITE FOLLOWING ACCEPTANCE OF GRADING CONDITIONS. REFER TO GRADING PLAN FOR CONTOURS AND SPOT ELEVATIONS.
- SITE CLEAN-UP SHALL BE PERFORMED ON A DAILY BASIS. SIDEWALKS, PARKING LOTS, ROADWAYS, ETC. SHALL BE KEPT CLEAN AT ALL TIMES.
- EXCESS AND WASTE MATERIALS SHALL BE DISPOSED OFF-SITE IN ACCORDANCE WITH APPLICABLE GOVERNMENTAL REGULATIONS.
- CONTRACTOR IS RESPONSIBLE FOR ALL DAMAGE DUE TO OPERATIONS INSIDE AND OUTSIDE OF THE CONTRACT LIMIT LINE. ANY AREAS OUTSIDE THE LIMIT OF WORK THAT ARE DISTURBED SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE OWNER.
- ALL TREES, SHRUBS, AND PERENNIAL PLANTS ARE LISTED IN THIS PLANT SCHEDULE. IF DISCREPANCIES EXIST BETWEEN QUANTITIES SHOWN ON THE DRAWINGS AND QUANTITIES LISTED IN THE PLANT SCHEDULE, THE PLAN QUANTITIES SHALL PREVAIL.
- PLANT LOCATIONS SHOWN ON PLANS ARE REPRESENTATIONAL ONLY. FIELD ADJUSTMENTS OF PROPOSED PLANT LOCATIONS MAY BE REQUIRED TO MINIMIZE POTENTIAL INTERFERENCE WITH EXISTING UTILITIES. TO MINIMIZE HAZARDS TO PLANT GROWTH AND TO IMPROVE MAINTENANCE CONDITIONS, PLANT LOCATIONS SHALL BE APPROVED BY THE OWNER'S REPRESENTATIVE PRIOR TO PLANT MATERIAL INSTALLATION.
- ALL PLANT MATERIALS MUST CONFORM TO AMERICAN STANDARDS FOR NURSERY STOCK (A.S.N.S.), LATEST EDITION PUBLISHED BY THE AMERICAN ASSOCIATION OF NURSERYMEN, WASHINGTON D.C. LARGER SIZED PLANT MATERIALS OF THE SPECIES LISTED MAY BE USED IF THE STOCK CONFORMS TO THE A.S.N.S.
- ALL LANDSCAPE ON SITE SHALL BE PER CITY OF WAREHAM CITY CODE LANDSCAPE REQUIREMENTS.
- ANY PROPOSED SUBSTITUTIONS OF PLANT SPECIES SHALL BE MADE WITH PLANTS OF EQUIVALENT OVERALL FORM, HEIGHT, BRANCHING HABIT, FLOWER, LEAF, COLOR, FRUIT AND CULTURE, AND ONLY AFTER WRITTEN APPROVAL OF THE LANDSCAPE ARCHITECT.
- THE CONTRACTOR SHALL SUBMIT DOCUMENTATION VERIFYING SEED LABEL AND PLANT CULTURE, SIZE, AND CONDITION, AS WELL AS NURSERY CERTIFICATION TO LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO INSTALLATION. THE OWNER AND/OR LANDSCAPE ARCHITECT RESERVE THE RIGHT TO REJECT PLANTS THAT DO NOT CONFORM TO PROJECT SPECIFICATIONS AND ANSI STANDARDS UPON DELIVERY TO THE SITE.
- ALL PLANT MATERIALS SHALL BE PLANTED WITHIN 48 HOURS OF ARRIVAL ON-SITE.
- THE CONTRACTOR SHALL REMOVE ALL CONSTRUCTION DEBRIS AND MATERIALS INJURIOUS TO PLANT GROWTH FROM PLANTING PITS AND BEDS PRIOR TO BACKFILLING WITH PLANTING SOIL MIX.
- ALL PLANTING BEDS SHALL RECEIVE 12" DEPTH IMPORTED PLANTING SOIL MIX.
- PROVIDE A SPADE-CUT EDGE AT THE PERIMETER OF ALL MULCHED PLANTING BEDS UNLESS OTHERWISE NOTED, SEE DETAIL.
- STEEL EDGING SHALL BE PROVIDED AT ROCK BED AND FENCE LINE BED. EDGING SHALL BE SURE-LOC BLACK COATED STEEL EDGING SIZED 1/8" X 4" X 16" OR APPROVED EQUIVALENT.
- APPLY PRE-EMERGENT WEED PREVENTER TO PLANTING AREAS PRIOR TO MULCHING. TOP-DRESS PLANTING AREAS WITH DOUBLE-SHREDDED HARDWOOD MULCH TO A DEPTH OF 3".
- SEED ALL DISTURBED AREAS UNLESS OTHERWISE NOTED. SEED MIX SHALL BE PER MASSACHUSETTS DOT SPECIFICATIONS AND SHALL BE IN ACCORDANCE WITH APPROPRIATE SEEDING DATES AND SHALL BE MIX SPECIFIED UNLESS OTHERWISE APPROVED.
- ALL SOD AREAS SHALL RECEIVE A MINIMUM 8" DEPTH OF TOPSOIL. TILL TOPSOIL INTO IN-SITU SOILS TO A DEPTH OF 4".
- ALL PLANT MATERIAL SHALL BE GUARANTEED TO BE IN A LIVE AND HEALTHY GROWING CONDITION FOR TWO FULL GROWING SEASONS (TWO YEARS) AFTER FINAL PROJECT ACCEPTANCE OR SHALL BE REPLACED FREE OF CHARGE, BY CONTRACTOR, WITH THE SAME GRADE AND SPECIES.
- EXISTING TREE LINE SHALL BE PROTECTED IN ALL AREAS AS REQUIRED UNLESS OTHERWISE NOTED OR APPROVED BY THE OWNER OR OWNERS REPRESENTATIVE. ANY EXISTING TREES TO BE REMOVED ALONG DOTY STREET MUST BE APPROVED PRIOR TO REMOVAL AND REPLACED AS NECESSARY FOR CITY REQUIREMENT BUFFER YARD STANDARDS. PROPOSED TREE SPECIES AND QUANTITIES SHALL BE APPROVED BY OWNER AND/OR OWNERS REPRESENTATIVE PRIOR TO ORDERING AND INSTALLATION AND SHALL BE SUBJECT TO ALL PLANTING STANDARDS REQUIREMENT FOR THE SITE.
- SEE CURRENT CONDITION PHOTOS ON SHEET C402

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JEFFREY SQUIRE, RLA 1549 07/05/2022

EVERSOURCE WAREHAM
VEHICLE STORAGE LOT
EVERSOURCE ENERGY
DOTY STREET, WAREHAM, MA 02576

PRELIMINARY
- NOT FOR
CONSTRUCTION

LANDSCAPE PLAN
C401

DRAWN: SLB
APPROVED: JDS
ISSUED FOR: PERMIT RESUBMITTAL
DATE: 07/05/2022
PROJECT NO: 211970
FIELD BOOK: ---
CLIENT NO: ---



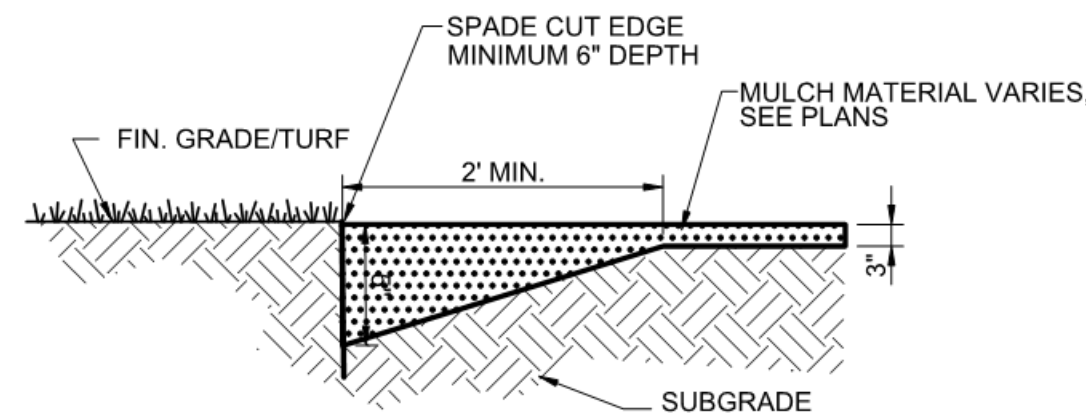
DOTY STREET LOOKING NORTH EAST



DOTY STREET LOOKING NORTH WEST

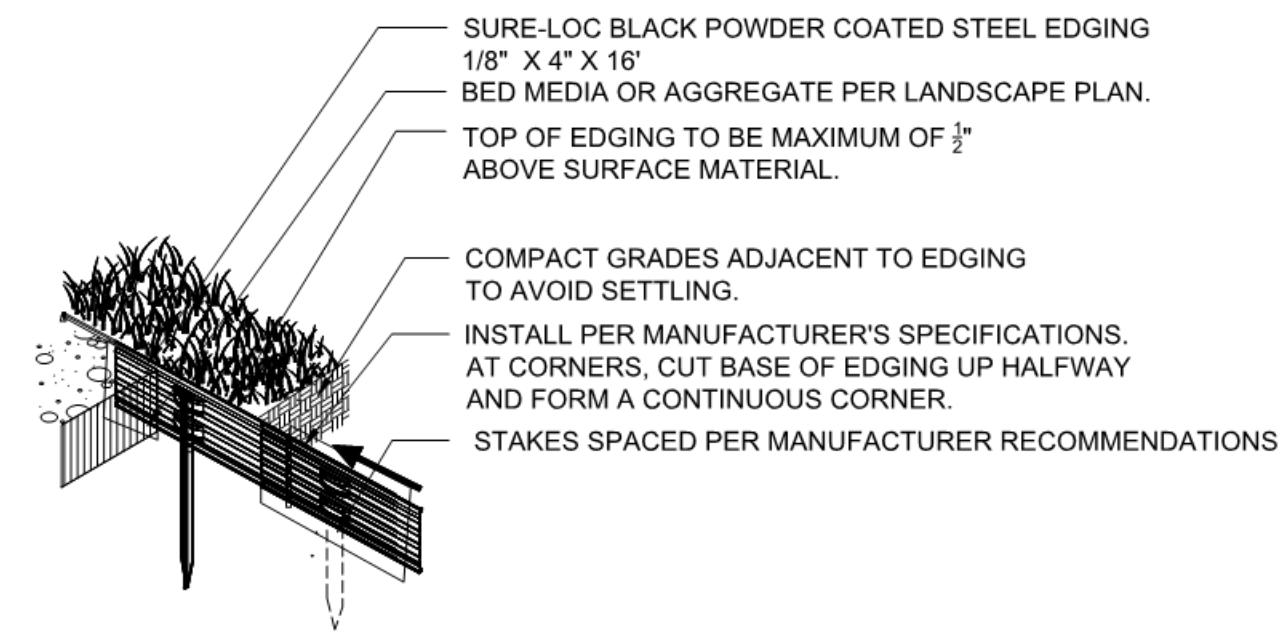
9 EXISTING CONDITIONS PHOTOS - 5 DOTY STREET

NOT TO SCALE



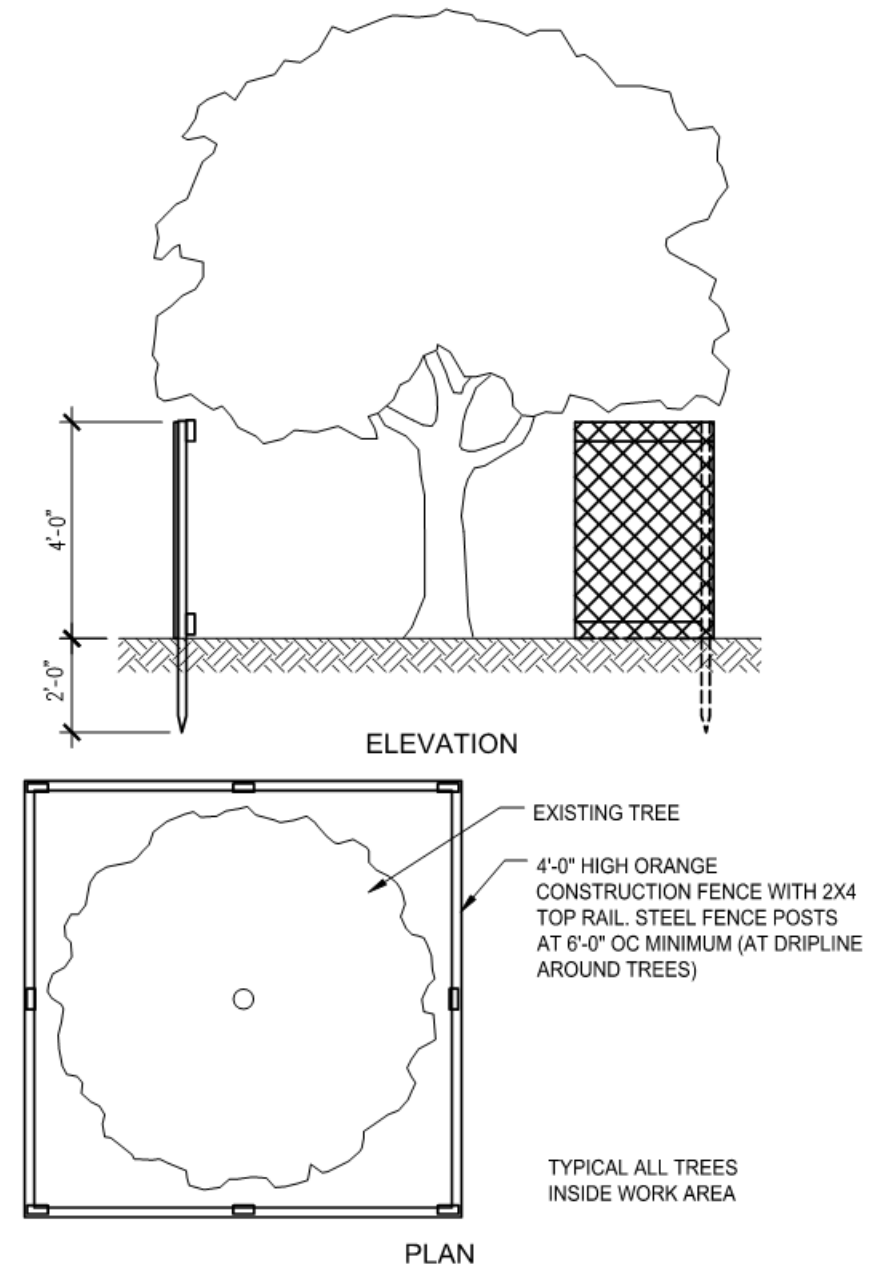
8 TYPICAL SPADE EDGE AND MULCH DETAIL

NOT TO SCALE



7 METAL EDGING

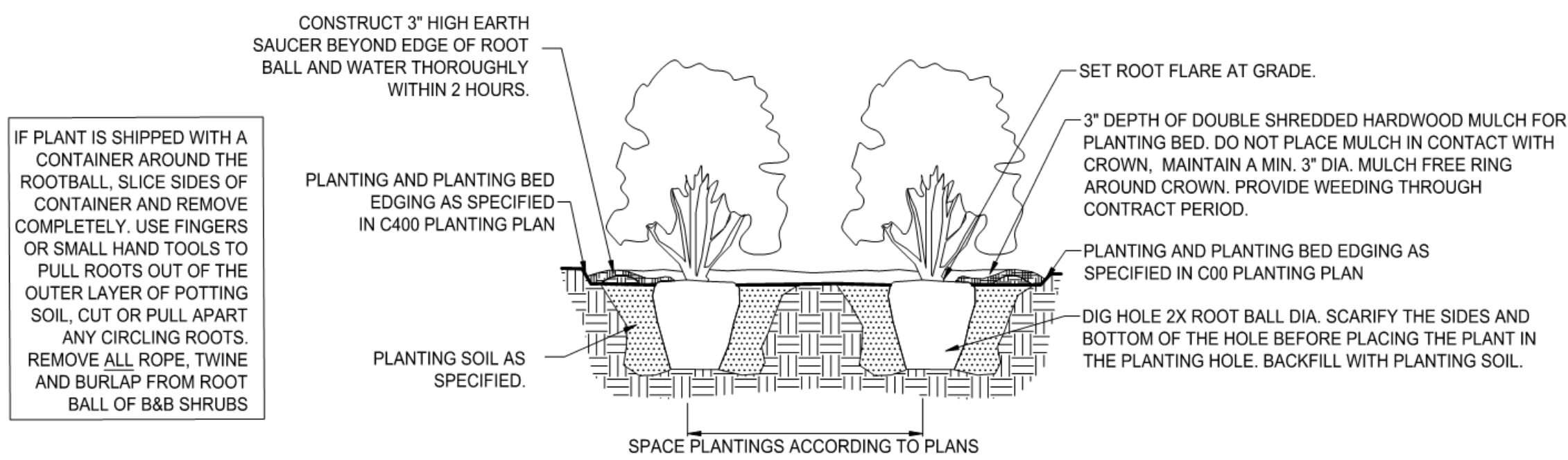
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6 TREE PROTECTION DETAIL

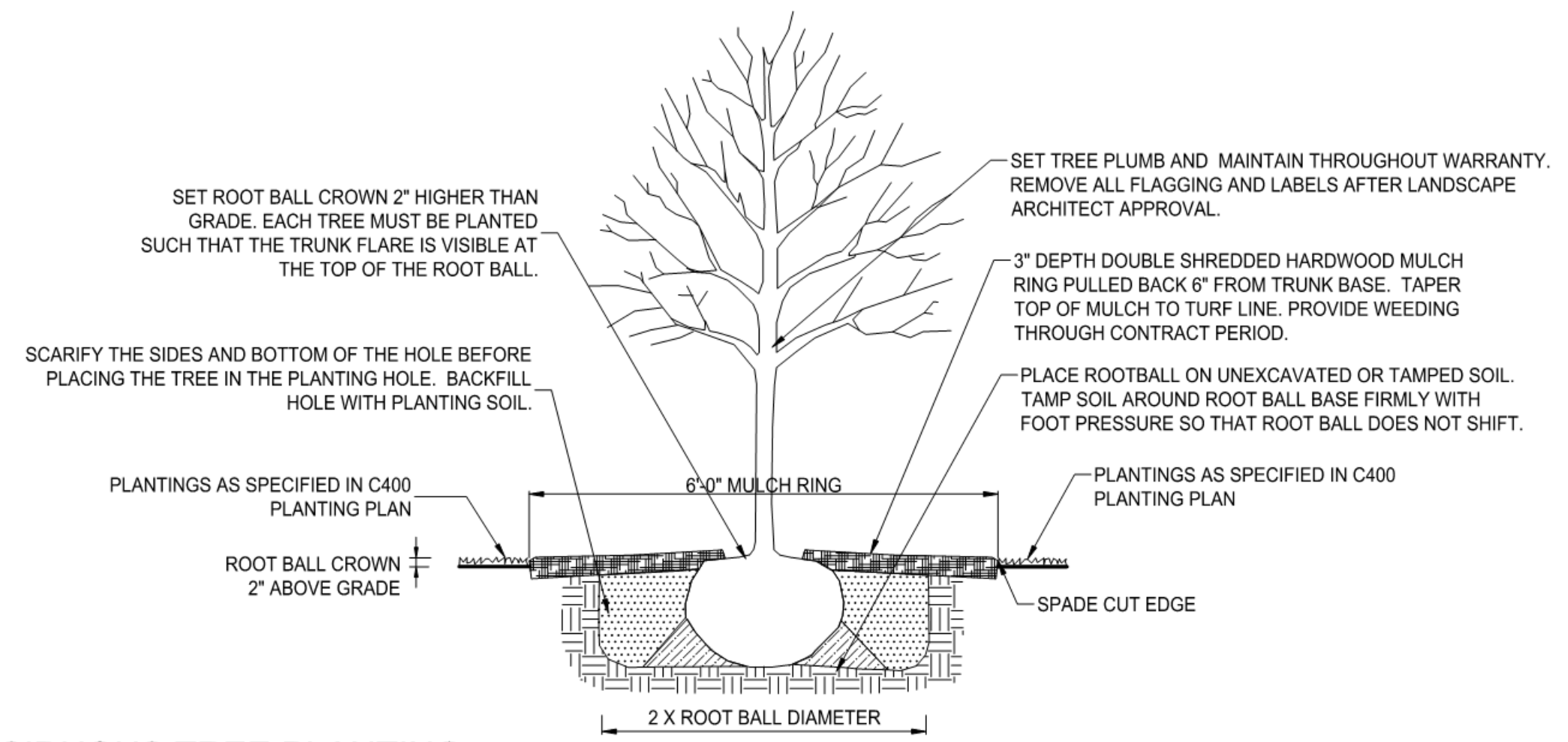
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5 SHRUB AND PERENNIAL PLANTING AND EDGING

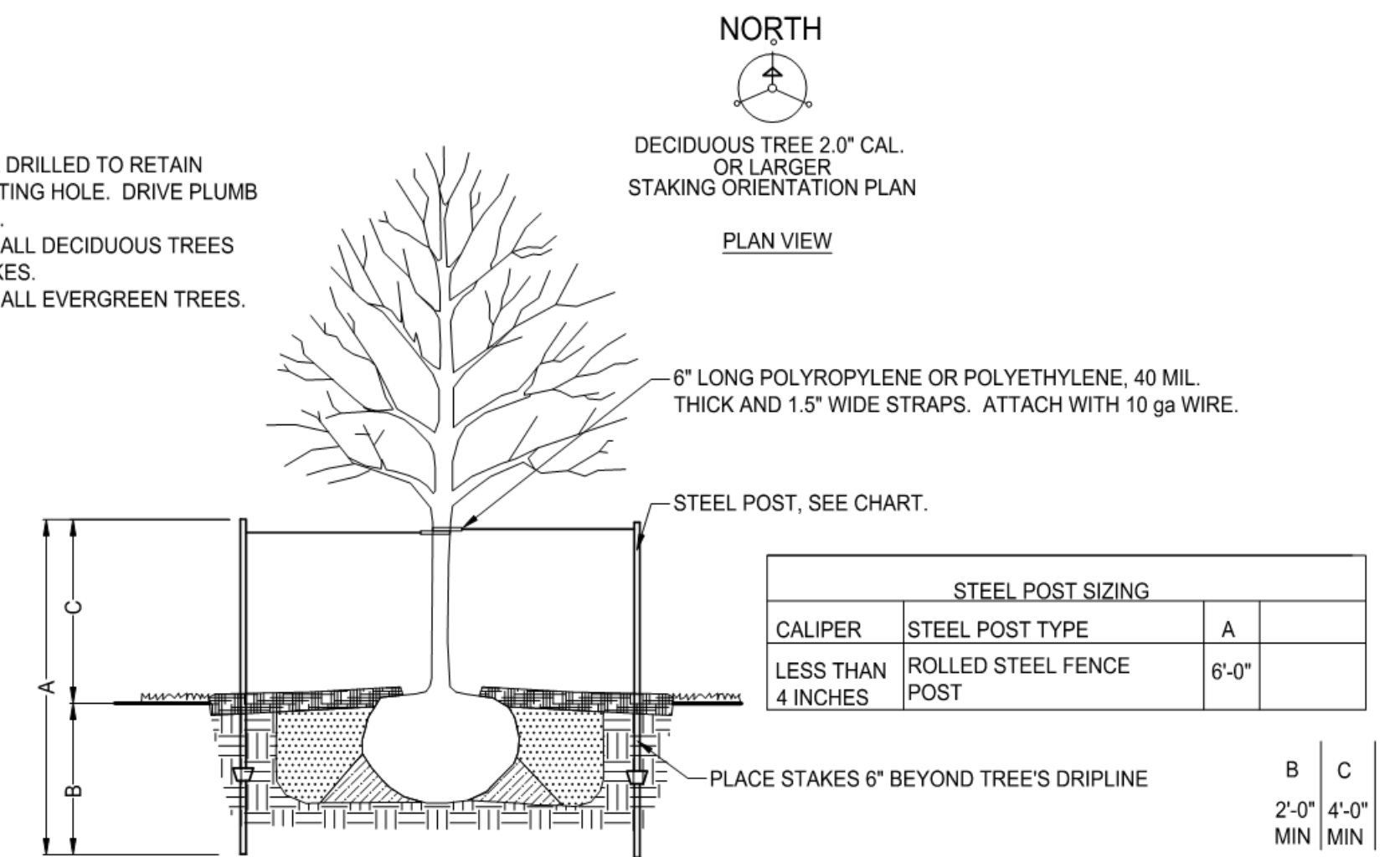
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1 DECIDUOUS TREE PLANTING

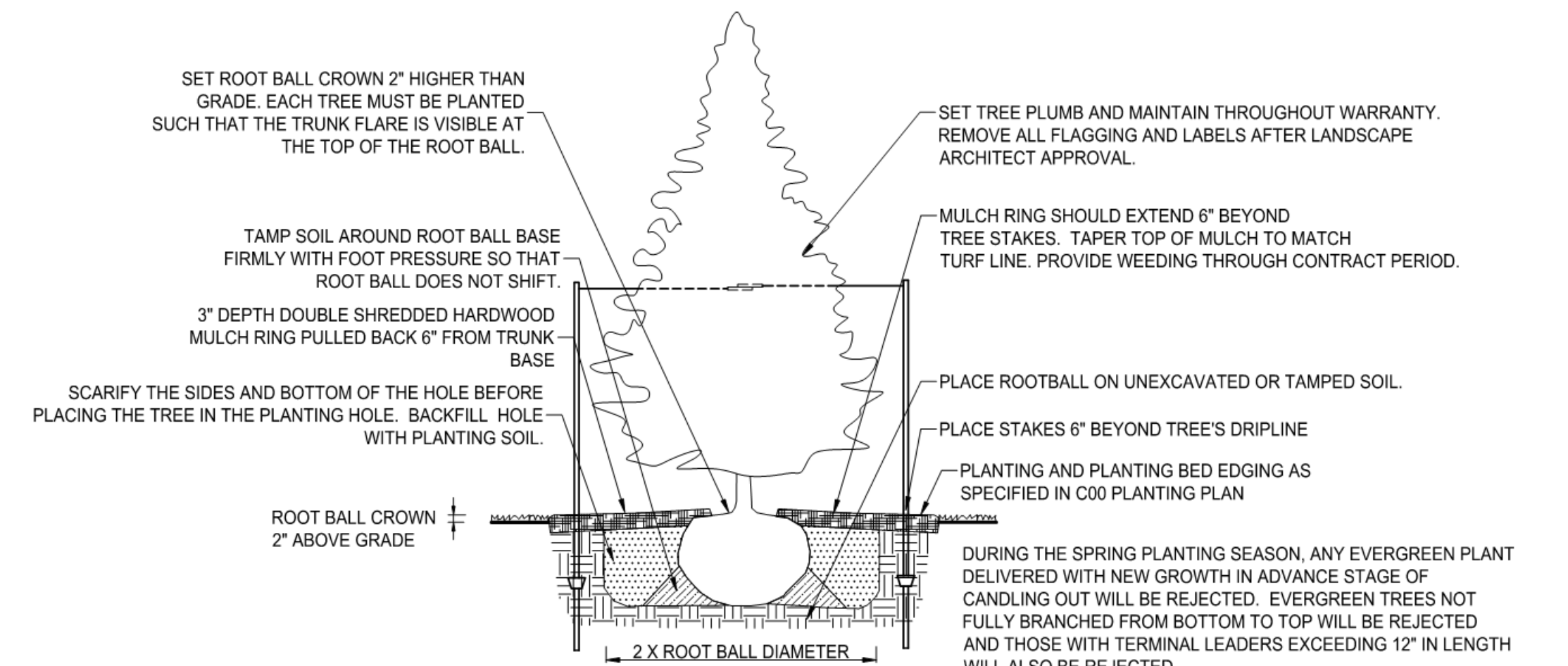
NOT TO SCALE

- NOTES:
1. STEEL POSTS TO BE NOTCHED OR DRILLED TO RETAIN WIRES. PLACE OUTSIDE OF PLANTING HOLE. DRIVE PLUMB REGARDLESS OF GROUND SLOPE.
 2. TREE STAKING IS REQUIRED FOR ALL DECIDUOUS TREES 2.0" CAL. OR LARGER. USE 3 STAKES.
 3. TREE STAKING IS REQUIRED FOR ALL EVERGREEN TREES. USE 2 STAKES.
 4. REMOVE AT THE END OF 1 YEAR.



2 TREE STAKING

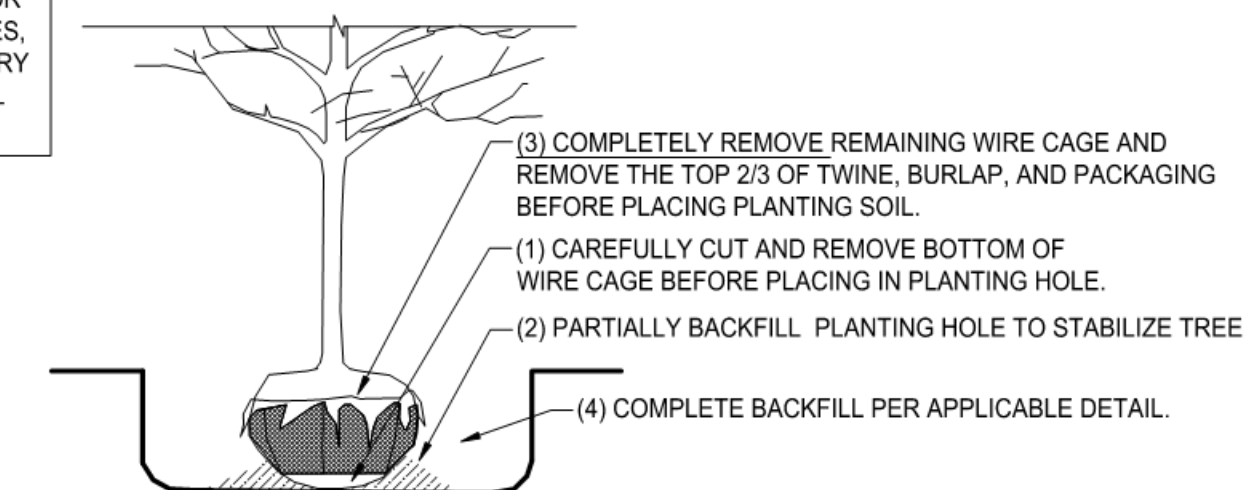
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3 CONIFEROUS TREE PLANTING

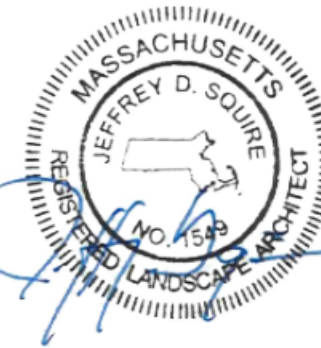
NOT TO SCALE

PLANTING NOTE:
DURING ESTABLISHMENT PERIOD THE CONTRACTOR SHALL PROVIDE WEEDING, ADJUSTMENT OF STAKES, REPAIR OF WATER SAUCERS AND WORK NECESSARY TO MAINTAIN PLANTS. WORK SHALL BE INCIDENTAL TO COST OF PLANTS.



4 ROOTBALL INSTALLATION

NOT TO SCALE



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